

# PLANNING POLICY

*"WE HAVE TO REVITALIZE THE TOWN THROUGH REVITALIZING CITIZENS' THINKING. THE WHOLE IDEA BEHIND THESE BOOKS IS TO MAKE PEOPLE AWARE OF WHAT THE TOWN USED TO BE AND WHAT IT COULD BE. YOU HAVE TO KNOW THE PAST TO MOVE FORWARD."  
-ALICE LIENARD THOMASSY*

The intent of these design guidelines is to promote and encourage development in Bridgewater, but in a manner consistent with the Town's historic character and unique development patterns. Unchecked development can create unrecognizable towns that do not have a strong sense of place or authentic character. By recognizing, celebrating, and capitalizing upon Bridgewater's unique architectural story, these guidelines can help the Town continue to develop into a beautiful and economically viable community.

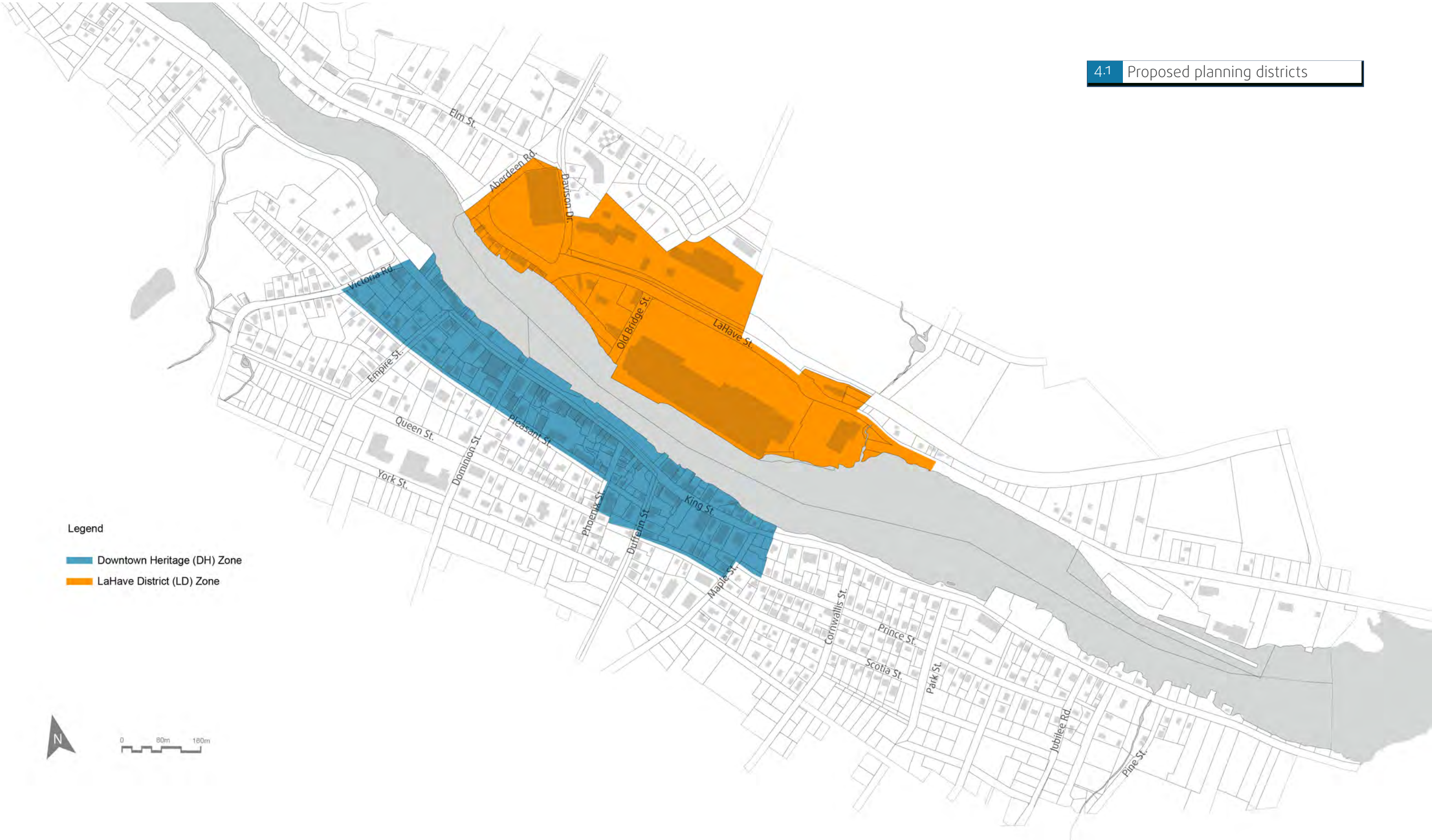
For the purposes of the guidelines, two unique areas of downtown have been identified. The first is the historic area centered along King Street, southwest of the LaHave River. This downtown area includes adjacent streets and large historic residential swaths. The second area is the LaHave District northeast of the River, centered around LaHave Street and the existing Bridgewater Mall. This area is currently dominated by newer retail buildings, but has enormous potential for future development.

Each area requires slightly different approaches to the details of design. However, there are many design principles common to each, as follows:

- » Reinforce the streetscape and create walkable environments;
- » All new structures should be oriented to the street;
- » Corner structures must address both street frontages;
- » Retail space must be transparent and highly articulated;
- » Weather protection to pedestrians (awning, porticos, etc.) should be provided;
- » Street wall should be in scale to the pedestrian environment;
- » Active transportation linkages should be an integrated aspect of the streetscape, including bike lanes and multi-use trails where possible;
- » The pedestrian environment should be respected. Safe distances between pedestrian and vehicular traffic should be maintained;
- » Where appropriate sidewalks should provide occasion for lingering through the use of street furniture, plantings, shade trees, and comfortable distances between buildings and streets.;

4.1 Proposed planning districts

- Legend
- Downtown Heritage (DH) Zone
  - LaHave District (LD) Zone



## 4.1 Downtown Architecture

While the downtown is dominated by what can be described as “Maritime Vernacular,” the residential areas also host interesting examples of Colonial, Gothic Revival, Queen Anne, Romanesque, and Second Empire, as well as a few buildings constructed in the Greek Revival, and Neo-Classical styles which came in and out of fashion during the past few centuries.

These styles each have their own set of geometries, proportions, and fenestrations, and with a careful eye and a bit of practice they can be easily spotted throughout town.

- **Maritime Vernacular** (1830s – 1900) has precedents in the Colonial Style era and relies heavily upon natural stone or brick for base structure, and local woods for interior timbers, walls, interiors, and exteriors. The exterior is unadorned with minimal trim, sports a shingle or clapboard siding, and is usually 1 ½ stories in height, with a modest number of plain dormer windows.
- **Colonial Style** (1700-1900) is a richer, more detailed, version of the Maritime Vernacular. Construction materials and patterns are very similar, but feature more refined details such as casement style windows, steeper roofs, richer ornamentation, and often large central hearths and tall chimneys.
- **Gothic Revival** (1850 – 1870) emphasizes the steeply pitched roof and the vertical line. Asymmetry is common, with most structures being 2 ½ stories in height, the roofline dominated by gabled dormer windows. Points and arches over windows and doors are highly emphasized, as are steeply pitched rooflines. The exteriors are shake, clapboard, or shingle, with an often ornate exterior of ‘gingerbread’ style fenestration.
- **Queen Anne Revival** (1885 – 1900) are asymmetrical houses often including an off-centered tower structure with a high pitched roof, and wrap around verandas. Steeply pitched roofs with gabled dormers, prominent chimneys, and a heavy use of oriel windows are all hallmarks of the style.
- **Romanesque Revival** (1880 – 1900) This style of building is mostly reserved for public and civic structures as the initial construction costs are much higher due to a heavy use of masonry and brick. The geometries are heavy, with wide arched windows and doorways, ornate brick and stone detailing, asymmetry of floor plan, and the liberal use of towers, circular windows, peaked gables, arches, and other old-world detailing.
- **Second Empire** (1865 – 1880) draws heavily from French influences and sports the iconic mansard roofline. Asymmetry is not uncommon, but most buildings constructed in this style are symmetrical with a central tower, and heavily gabled wings with jutting bays on either end. Copper or iron decorations are common.



5.2 Colonial



5.3 Maritime Vernacular



5.4 Gothic Revival



5.5 Queen Anne Revival



4.6 Romanesque Revival



4.7 Second Empire

- **Greek Revival** (1830 – 1860) revives the forms of Greek antiquity with an emphasis on balance and proportion, symmetry, straight lines, central doors, and overall simple geometries. Antiquity is further represented through the use of classically detailed pediments, columns, pilasters, transoms, etc.
- **Neo-Classical** (1810 – 1830) is best summed up as Greek Revival with Baroque touches. Proportions are balanced, symmetry is the norm, roof pitches are low, all in accord with Greek Revival. However, additional articulation of ornate filigree or statuary is glazed over the classical base

New construction in Bridgewater does not need to replicate historic styles, in fact it is preferred if replications are discouraged. However, new construction should be of a definitive and compatible architectural style where lines, patterns, and forms are harmonious and consistent. A well articulated integration of new and old built form will create a thoroughly timelessness and authentic streetscape and a strong urban character.



4.8 Neoclassical



4.9 Greek Revival



4.10 Integration of old & new styles

## 4.2 Historic Downtown

Within the Historic Downtown section there are unique design guidelines which apply to the residential, commercial, and civic architecture.

- » Single Unit Residential shall not be constructed along either side of King Street within the Historical Downtown zone.
- » Buildings along King Street should be constructed to the lot line.
- » Buildings on King Street should be minimum of two or three storeys in height.
- » Maintain existing rhythm of facade width and spacing between buildings.
- » Buildings on steep slopes should have a walkout floor plan.
- » All buildings should be designed to an architectural style that is compatible with the Town's existing architectural forms (Maritime, Queen Anne, Victorian, Craftsman, Colonial, Second Empire, etc).
- » The architectural style of new buildings should respect the historic rhythm and form of existing architecture, but in a new and contemporary style. Historical reproductions are not desirable, but neither are buildings which are in marked aesthetic contrast to the neighbourhood.
- » Each new building should have a clearly identifiable style. Windows, doors, roofs, porches, and other details should reinforce the style.



4.11 Bank of Montreal Building



4.12 Rofhese's Building



4.12 Old Post Office

## Semi-detached and Townhouse

### General Principles:

- » For new development in the Downtown, an increase in density through the construction of semi-detached homes and townhouses is acceptable in all areas of the zone, except for King Street.
- » Semi-detached and townhouse development is not permitted on King Street.
- » The quality of new residential development must be high.
- » New buildings must demonstrate a definitive architectural style consistent with the historic nature of the area.

### Guidelines

- » All buildings should be designed to an architectural style that is compatible with the Town's architectural forms (Maritime, Queen Anne, Victorian, Craftsman, Colonial, Second Empire, etc). Each building should have a clearly identifiable style. Windows, doors, roofs, porches, and other details should reinforce the style.
- » Dwellings on corner and flanking lots should be designed so both exposed facades are oriented towards the street. There should be no blank side walls or gables.
- » Garages are not encouraged, but when necessary only single car garages less than 3.25 metres wide are permitted.
- » No garage should project beyond the front building facade.
- » Porches are encouraged on all semi-detached homes and townhouses, and should wrap around corner units.
- » Parking is encouraged in the rear or side of the building instead of in the front (when practical). On street parking is a desirable alternative.
- » Low flow fixtures are encouraged.
- » Fireplaces are encouraged. Chimneys often add to the character of townhouses and semi-detached homes.
- » Fences in the front yard are not permitted.
- » Each unit should have a landscape bed occupying at least 25% of the front yard if the building is set back more than 2 metres from the property line.
- » Front setbacks cannot exceed 7 metres from the centre line of the road.
- » All driveways must be concrete, brick, or pavers. No net increase in surface run-off is permitted.
- » All stormwater run-off must be managed onsite, through rain barrels or rain gardens.
- » No water or electrical meters are permitted on the front of a building.

4.13 Town Home Development



## Mid-Rise Apartment Buildings and Condos

### General Principles:

- » For new development in the Historic Downtown, the increased density of apartments or condominiums is preferred over single detached, semi-detached, or townhouses.
- » All mid-rise developments must have an active ground floor use that relates to the streetscape. Ground floor residential is not permitted in a multi-unit building.
- » The quality of new residential development must be high.
- » New buildings must demonstrate a definitive architectural style.
- » New buildings must be 'street-related'.
- » New buildings should not exceed 6 stories.

### Guidelines

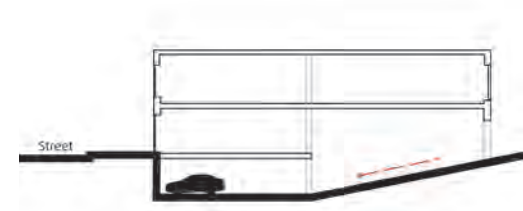
- » All buildings greater than four units should be designed by an architect registered to practice in the Province of Nova Scotia.
- » All buildings should be designed to an architectural style that is compatible with the Town's architectural forms (Maritime, Queen Anne, Victorian, Craftsman, Colonial, Second Empire). Each building should have a clearly identifiable style. Windows, doors, roofs, porches, chimneys & other details should reinforce the style.

4.14 Mid Rise Multi-Family



### Site

- » Locate parking either internally to the building, underground, or in behind. Street-facing surface parking for multi-unit residential is not permitted.
- » Ensure that driveways for vehicular and service access have minimal impact on the streetscape. Ideally, service areas should be behind or internal to the building. Driveways should be a maximum width of 6 metres.
- » No net increase in surface run-off is permitted.

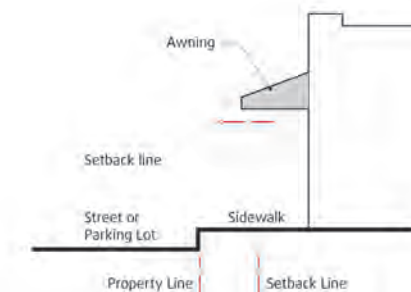


### Height & Massing

- » New apartment or condominium buildings should be no less than 2 storeys and no more than 6 stories.
- » Buildings of 2 storeys should have an architecturally distinct base (first and second storey), and top (roof).
- » Buildings of 3-6 storeys should have an architecturally distinct base of 1 or 2 stories, a distinct middle, and the upper floor shall be incorporated into the roof design (i.e. the roof eave is no higher than the 1.2 metres above the finished floor elevation, with gabled windows allowing an occupy-able space.).
- » Buildings over 3 storeys should incorporate a 2 metre minimum 'step back' from the front facade to minimize the building's mass to pedestrians on the street.
- » Corner buildings should incorporate articulation and forms that emphasize the importance of the corner.

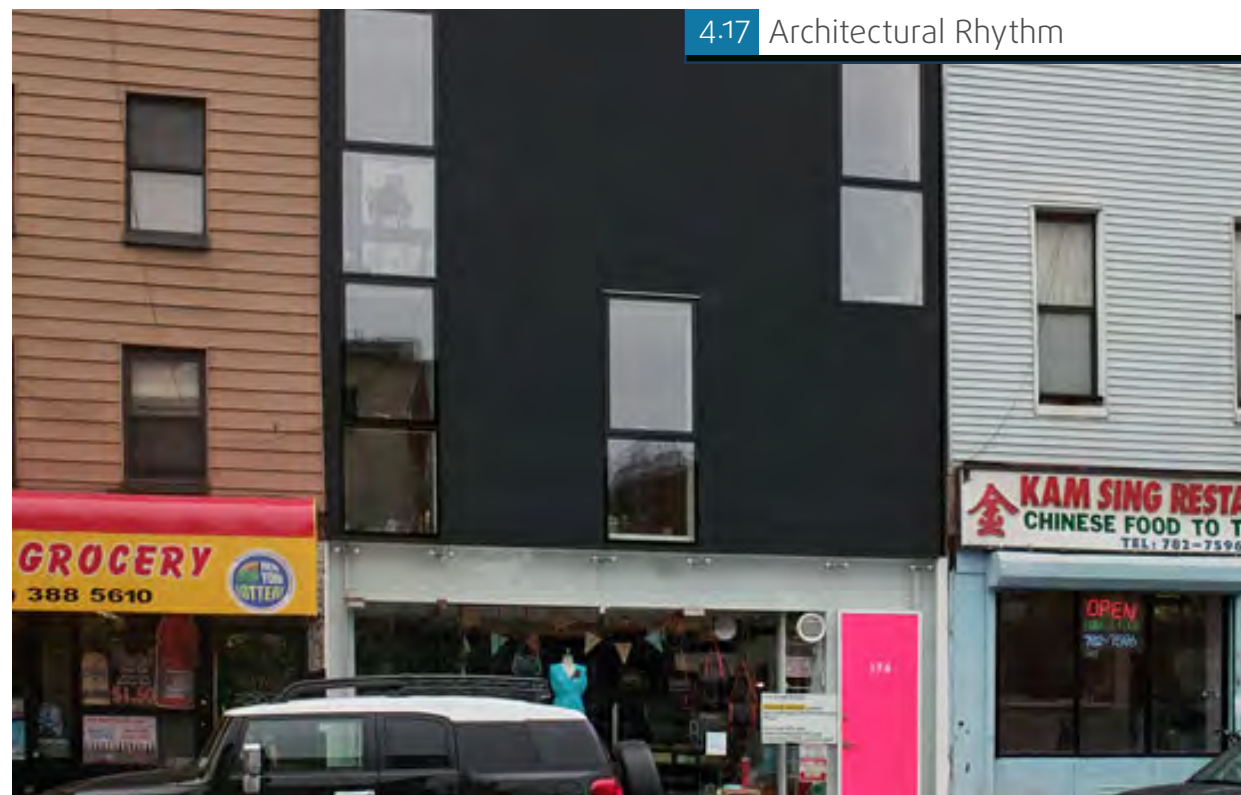


4.15 Mid Rise Multi-Family Mixed Use





4.16 Scaled Entry Plaza



4.17 Architectural Rhythm

## Entries

- » Primary entries to multi-unit buildings must front on an adjacent street.
- » All buildings should have a high quality entry plaza no less than 10 square metres as part of the streetscape between the sidewalk and the building entry.
- » At a minimum, the entry plaza should be 1 metre in depth by the 10 metres in length.
- » The entry plaza must provide seating, either by a seat wall or site furnishings. The plaza should be made of pavers, stone, or concrete (or a combination of).
- » The entry should be a highly visible architectural feature incorporating elements such as awnings, roof overhangs, lighting, double doors, glazing and/or distinct architectural materials.

## Details

- » Chimneys often add to the character of multi-units building and are encouraged as part of the architectural decoration.
- » Mechanical or communication appurtenances larger than 0.50 square metres should not be visible from the street. Loading and service areas should not occupy more than 6 metres of building or street frontage, and should be screened from view by enclosure. These areas should be designed as an integral part of the building and made of finished materials compatible with the design of the primary building.
- » Awnings or overhangs are required to cover at least 20% of the ground-floor facade. Overhangs must have at least 2.7 metres of clearance for snow clearing, may not project more than 2/3 of the width of the sidewalk or a maximum of 3 metres, and should be no wider than a single storefront or architectural bay, whichever is narrower.
- » No upper storey projecting patios are permitted facing the street. All patios must be recessed into the building at least 1.5 metres, with no patio extending more than 2.0 metres beyond the facade. Any patio facing the street must be screened using the prevalent facade material (brick, shingles, steel, glass, etc.) to at least 0.75 metres above the floor elevation.
- » The bottom of any patio, as a visible element of the building, must be afforded the same scrutiny as the siding material on the building. Plain concrete finish or wood decks are not permitted.
- » Buildings greater than 12 metres of frontage length need to be broken down into horizontal compartments with a distinct change in vertical architectural articulation using different facade materials, projections, bays, roof changes, colours, etc.
- » All private balconies will have a retractable laundry line installed as a standard feature.
- » Rooftop mechanical equipment and vents should be incorporated as an integral part of the building design wherever possible. Roof top units and vents should be set back from the roof edge and screened using materials complementary to the building.

## Ground Floor Mixed Use

### General Principles

- » All mixed-use buildings in the Downtown District should have an active commercial or institutional ground-floor.
- » Ground-Floor residential is not permitted along King Street in this district. Entry ways to upper storey residential uses are permitted.
- » Bicycle parking facilities should be provided to encourage alternative modes of transport, particularly for employees and consumers with small purchases
- » All previous design guidelines for Mid-Rise Apartment Buildings and Condos apply to Ground-floor Mixed-Use except for the following ground-floor guidelines:

4.18 Mixed Use



### Built Form

- » Main entries shall be located along primary streets, with prominent corner entry to buildings located at street intersections.
- » Windows and doors shall occupy no less than 60% of the ground floor facade area for buildings fronting on streets between a height of 1.2 and 2.0 meters above grade. The use of clear glass (at least 88% light transmission) on first floor storefronts is recommended. Reflective or tinted glazing should not be used, however passive shading devices such as "special E" glass technologies, deep overhangs, or external sun shades and trees may be used.
- » Any ground-level retail windows shall be kept visible at night (i.e. unshuttered).
- » If a facade extends along a sidewalk, no more than 40% of its length, or 15 metres, whichever is smaller, may be blank.
- » A wall with a green mural is not considered a blank wall.
- » Building Setback: The building setbacks should be no more than 1 metre from the front lot line for at least 60% of length of the facade. No part of the ground floor facade shall have a setback greater than 2.5 metres.
- » The use of window mullions or recessed windows, set into the facade, will create enhanced shadow lines. Shutters, operable or inoperable, should be sized to the window they are associated with such that, if closed, they cover the entire window.
- » Doors for retail shops should include a high percentage of glass area. Security grilles along street frontages are discouraged unless integrated within the overall architectural theme.
- » Primary building entrances should directly front onto, and connect to, public sidewalks or open spaces. Secondary entrances may face interior plazas and open spaces, alleys, and surface and parking lots.
- » Overhangs or protected shelters are required over all public entry ways into multi-unit developments.
- » Architectural lighting should wash the entire first storey storefront at night.
- » The location of service area entrances along roads should be discouraged where possible. It is preferable to locate service areas off side streets or service driveways.



4.27 Corner Prominence



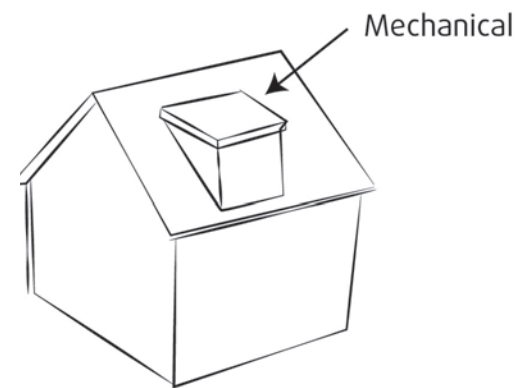
4.26 Zero Lot Line



4.25 Green Mural Program



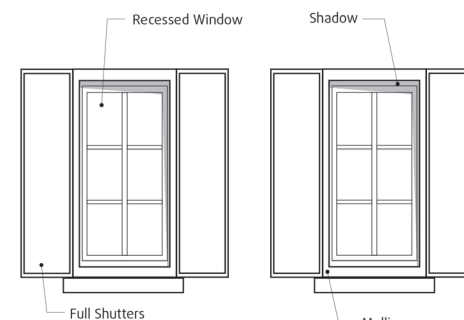
4.19 Bike Parking



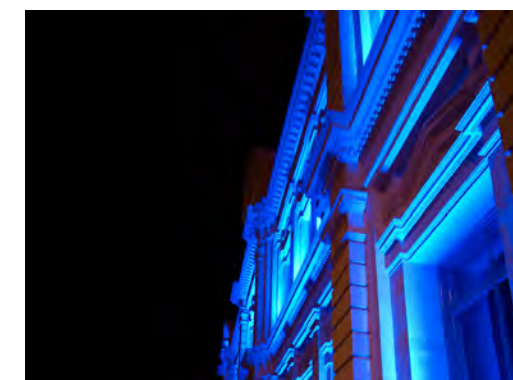
4.20 Hidden Mechanical



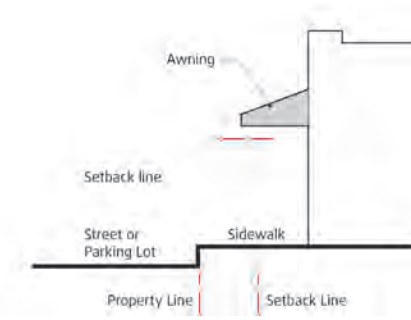
4.21 Awnings



4.22 Window Detail



4.23 Architectural Lighting



4.24 Covered Walkways

## Large Format Commercial

### General Principles

- » Strip malls, box retail, and large format commercial are not permitted in the historic downtown

## Institutional Uses

### General Principles:

- » Institutional uses, particularly government offices, are highly desirable Downtown.
- » Where possible, institutional uses which can renovate existing heritage buildings are strongly encouraged.
- » Institutional buildings should be designed as iconic features of the Downtown.
- » No net increase in surface run off is permitted.

### Guidelines

- » All Institutional buildings should be designed by an architect registered to practice in the Province of Nova Scotia.
- » All Institutional buildings should be designed to an architectural style that is compatible with the Town's architectural forms (Maritime, Queen Anne, Victorian, Craftsman, Colonial, Second Empire, Richardsonian Romanesque, modern, etc.). Each building should have a clearly identifiable style. Windows, doors, roofs, porches, decks, chimneys and other details should reinforce the style.

4.28 Existing Institutional Use



- » Wherever possible, new institutional uses should be encouraged to renovate existing heritage buildings.
- » At least 10% natural stone should be incorporated into all Institutional buildings.

### Entries

- » Civic entries and plazas are an important part of institutional buildings. Civic entries and plazas should incorporate an area of no less than 8% of the building's footprint.
- » Primary entries to institutional buildings must front on an adjacent street.
- » All institutional buildings should have no more than a 3 metre setback from the street for no less than 30% of the facade frontage. The remainder of the ground-floor frontage cannot setback more than 5 metres from an adjacent street.
- » The entry plaza should have seating walls of no less than 1 metre in length for each 5 metres of frontage.
- » The plaza should be made of pavers, stone, or concrete (or a combination thereof).
- » The entry should be a highly visible architectural feature incorporating elements like porticoes, roof overhangs, lighting, inset doors, glazing and/or distinct architectural materials.

4.29 Potential Adaptive Reuse Site



## Surface Parking

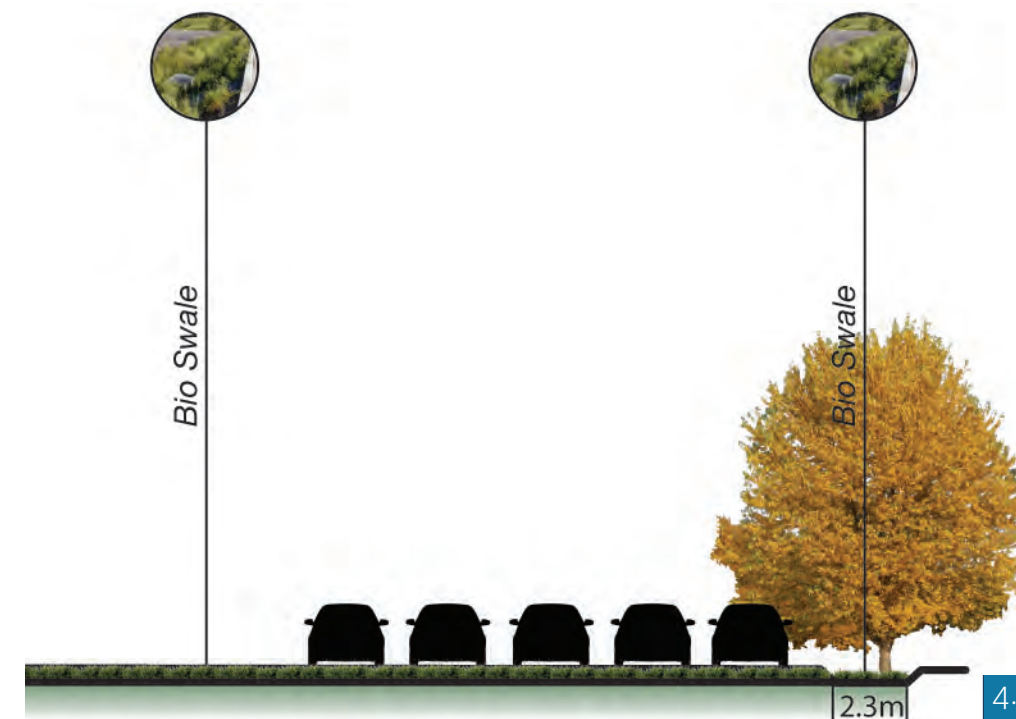
### General Principles

- » On-street parking should be provided on a minimum of 70% of both sides of King Street, where its presence does not obstruct traffic or compromise pedestrian or vehicular safety.
- » Off-Street Surface Parking should be configured and designed to reduce its visual dominance of from public streets.
- » Off-Street Surface Parking should incorporate high quality walkways and alleyways to connect parking lots to public streets and safely separate pedestrian and vehicle movements.
- » Off-Street Surface Parking should be designed to provide a strong visual quality through the use of high quality landscaping, lighting, and pavement materials.
- » Off-street parking should maximize capacity of rear lots by encouraging neighbouring land owners to work together to restructure and reorganize parking layout. The Town should continue to work with land owners to help facilitate this process.
- » Off-Street Surface Parking should be designed according to environmental sustainability principles, including the minimization of surface water runoff and 'heat island' effects.
- » No net increase in surface run-off is permitted.

### Guidelines

- » Entries to rear-lot parking shall be no greater than 6 metres in width.
- » No parking should be located between the street and the building
- » For parking fronting on any street in the Downtown, the parking frontage shall not exceed the building frontage
- » For every space which fronts on the street, the developer must install a 2 m wide landscape bed and provide at least one 50 mm caliper street tree. Planting to be approved by the Town prior to installation. A 1.5 m (minimum) concrete walkway must link the parking to the sidewalk.
- » A landscaped island should be located at each end of every parking aisle. Landscaped islands should be a minimum width of 2m wide and include one 50 mm caliper sized tree per parking row.
- » Limit the length of parking rows to a maximum 12 contiguous spaces per row. Longer spans should be broken up with landscaped islands (rain gardens). Ideally, all catch basins should be located at or near these islands.
- » Landscaped islands must be designed as stormwater catchment areas. Sub-base and base material are to be high performance granular material ideal for infiltration and interception of groundwater. When necessary, french drains or perforated-bottom catch basins will be installed to further facilitate infiltration.
- » Parking Islands will be curb-less, or have frequent curb cuts, and the surrounding area will be graded to drain towards the islands. Concrete curb stops should be used, if required.
- » Lighter asphalt is preferred over darker asphalt to reduce urban heat island effect. Asphalt should include a high percentage of white aggregate. Concrete is preferred over asphalt where practical.

4.30 Bioretention Island



4.31 Parking Layout

## Signage

### General Principles:

- » Signage should not be an afterthought in the building design but instead should be integrated into the design as a necessary element.
- » All signs should convey their message clearly and legibly, shall be vandal and weather resistant, and if illuminated shall not be overly bright for their surroundings.

### Sign Elements:

- » A sign board for commercial signage or awnings is required above at the top of the first storey and must be integrated into the architecture of the building.
- » Signs are permitted anywhere between the first and second floors.
- » Back-Lit or internally illuminated signs are not permitted anywhere in the Downtown area, except for backlight raised lettering only, where letters are no greater than 200 mm high and no deeper than 150 mm.
- » Projecting signs are strongly encouraged in the Downtown and should be designed to match the architectural style of the building or structure to which the sign is affixed as well as the signs and buildings adjacent and in the general area. Projecting signs can be no larger than 2 square metres. Projecting signs must have a clearance of 2.7 metres above grade.
- » No business shall exceed one wall or projecting sign per facade.
- » Wall mounted signs may not exceed 10% of the building's facade surface area.
- » Preferred materials for suspended or projecting signs include wood, metal, or a high-quality composite material.
- » Awning signs are encouraged along the street. Awning sign copy should be limited to the business name, or logo. The sign size should be in proportion with the awning. Awning graphics must complement the awning colour.

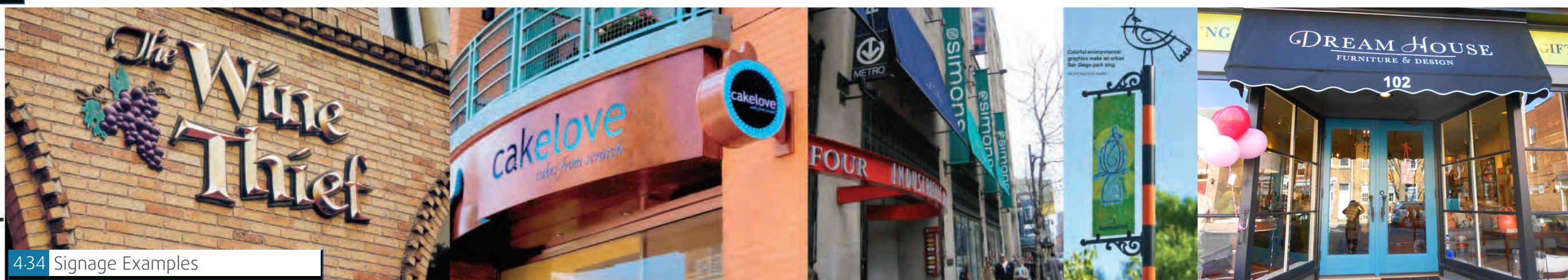
- » Spot lights, gooseneck light fixtures and other decorative light fixtures should be utilized for illuminating awning signs.
- » Buildings with businesses that do not have ground floor usable space may have one 2.5 square metre sign, such as a directory sign, to be utilized by all above ground floor businesses so long as those businesses do not also occupy ground floor usable space. Sign to be placed adjacent to the primary entrance for the above ground floor businesses.
- » New signs proposed for existing buildings should provide a compatible appearance with the building signage of other tenants. When there are multiple signs on a single building, there should be an effort to bring in a unifying element (such as size or material), even where no sign program exists.
- » Window signs are only permitted on the first floor level. Window signs shall be designed to attractively promote the business while still allowing for the customer to view the interior of the business. Letter height shall not exceed 200 mm.
- » Signs may be placed or painted on the interior or exterior glass windows or doors provided that such signs cover no more than 30 percent of the glass area of the entire storefront, nor more than 50 percent of the window in which it is placed.
- » Signage which contains business slogans or advertising is not permitted. However, signs may include information describing the products sold or services provided.
- » Temporary 'sandwich' board signs are permitted, to a maximum size of
- » Repetitious signage information on the same building frontage shall be avoided regardless of the sign area square footage allowed.
- » Temporary hand painted signs, vinyl signs, or vinyl lettering is not permitted.
- » Billboards, free-standing pole signs, monument signs, marquees, any kind of animation, roof, and signs painted on the exterior walls of buildings are prohibited. No flashing, traveling, animated, or intermittent lighting shall be on the exterior of any building whether such lighting is of temporary or long-term duration.
- » Modern low-energy LED lighting is required in all signs.



4.32 Signage Examples



4.33 Project Sign Area



4.34 Signage Examples

## Architectural Materials

The quality of the architecture will set the standard for the character and sense of place that evolves in the downtown. New developments should integrate with existing structures through consistent design and material quality, while encouraging contemporary and individualized architecture.

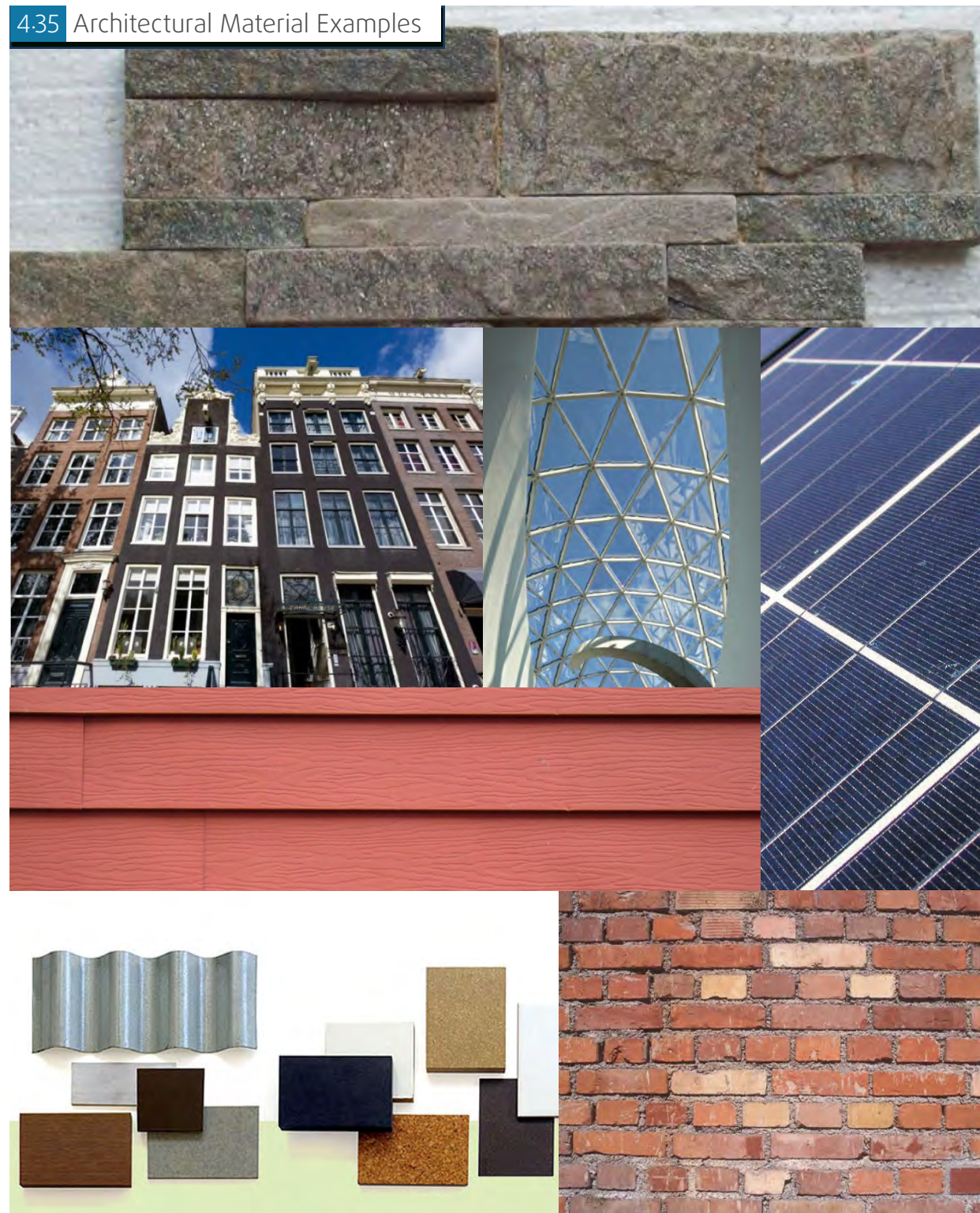
### General Principles:

- » These guidelines appeal to all new development or adaptive reuse projects in the Downtown.
- » Building materials for Downtown should be high quality.
- » Sustainable building practices are encouraged in the Historic Downtown, including the adoption of LEED-based principles or specific programs.
- » Materials used on the facade of any structure should be carried around the site if the streetwall is not continuous.

### Guidelines

- » The use of traditional, natural, high quality, durable materials which enhance the building's aesthetics and convey a sense of permanence shall be required. Green building materials, or materials produced in a sustainable manner are preferred.
- » Wood, cementitious board, red clay brick, high quality metal, glass and/or natural stone is encouraged as the primary exterior finish material. Aluminum siding, and vinyl siding are not permitted. Cladding materials may include brick, stone, metal, wood, glass, in-situ concrete, and pre-cast concrete (excluding split face concrete, or concrete block).
- » In general, the appearance of building materials should be true to their nature and should not mimic other materials. Any product that mimics a more expensive material is not permitted (e.g. stone tile, brick tile, etc). Vinyl siding, plastic, plywood, concrete block, mirrored glass and metal siding utilizing exposed fasteners are not permitted.
- » In general, heavier materials (like stone or metal) shall be used at the building's base, and lightest materials (glass or wood) shall be used at the building top.
- » Green building materials, solar panels, green walls, and other environmentally friendly building technologies shall be encouraged for all new developments or adaptive reuse.

4.35 Architectural Material Examples



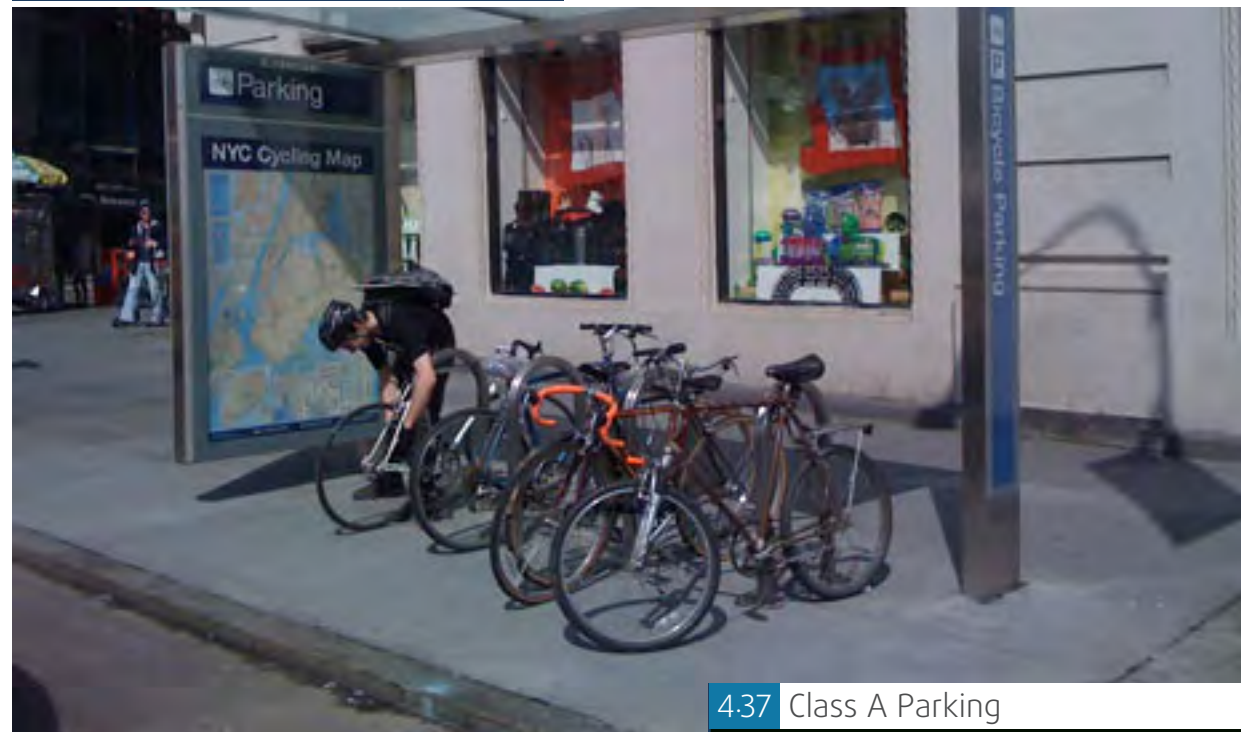
## Bicycle Parking

### Guidelines:

The minimum and maximum number of required bicycle parking spaces shall be as follows:

Use	Parking Requirement	
Multiple Unit Dwelling (four or more dwelling units)	0.5 spaces per dwelling unit.	80% class A, 20% class B
Hotels or Inns	1 space / 30 rooms.	80% class A, 20% class B
Commercial / Office	1 space / 500 sq.m GFA	20% class A, 80% class B
Entertainment	1 space / 25 seats	20% class A, 80% class B
Institutional Uses	1 space / 250 sq.m GFA	20% class A, 80% class B
Parking Garages	1 space / 20 parking stalls	Maximum of 50 bike spaces.
Any other use	1 space / 500 sq.m GFA	10% class A, 90% class B

4.36 Bicycle Parking Requirements



4.37 Class A Parking

### Class A Bicycle Parking means:

- » Covered parking spaces protected from precipitation within 50 meters of the building's primary entrance.
- » Parking spaces shall be highly visible, at ground level or monitored by constant security cameras. Parking areas to be well lit.
- » Directional signage shall be provided.
- » Parking spaces shall have a minimum overhead clearance of 2 metres.
- » Parking stalls shall be no less than 0.6 meters long and 1.2 meters high, and have an associated aisle of 1.5m in width. Door openings shall have an opening of 0.6 meters.

### Class B Bicycle Parking means:

- » Parking is located within 200m of the primary building entrance.
- » Parking spaces shall be no less than 0.6 meters long and 1.2 meters high, and have an associated aisle of 1.5m in width.
- » Bicycles should be supported at all times by a minimum of two attachment points.



4.38 Class B Parking

## 4.3 Heritage Preservation

### Design Principles

- » Balance Heritage Preservation & New Development: The preservation of heritage assets within the Downtown is a priority and supports the objectives for compact built form and enhancing community distinctiveness. Development sites situated adjacent to existing historic areas have general market appeal and should be developed as a catalyst for broad based urban renewal.
- » Character Sensitive: Respect the 'original' historic character and preserve original fabric where appropriate. Contemporary additions should also be considered.
- » Conservation: The alteration of any historical architectural feature or building should be resisted. The process and mechanisms for the conservation and restoration of heritage buildings should be prioritized.
- » Maximize Mixed Use & Reuse: Mixed-use development provides live/work opportunities, and contributes to vital, attractive Downtown character. Adaptive reuse should be considered as part of the conservation and restoration of heritage buildings.



### Guidelines

- » Where feasible, the original building fabric should be preserved. Repair rather than replace deteriorated architectural features. When replacement is necessary, match the original. Replacement of missing features should be based on historical accuracy.
- » Original elements that lie beneath contemporary alterations should be preserved and replicated where necessary if damaged.
- » Renovate shop fronts in keeping with the original building design, using those elements that are intact, and replacing missing features.
- » The base panel of heritage storefronts provides a visual and functional building base. Original base panels should be maintained when they exist with materials of the same colour and texture as the display window frame, or the plaster materials.
- » As the largest element of the shop front, the display window establishes the character of the facade. The continuity of large display windows should be maintained.
- » The display front is often divided near the top into transom windows. The transom should be maintained and any obstructions such as air conditioning units that have been inserted into this space should be removed.
- » Doors are a very important functional design feature of heritage buildings. The location, size, shape, and style of doors establish our most immediate impression of a building. Traditionally constructed of wood and glass and fitted with metal hardware, each style of door was designed to fit a particular style of building. The style of heritage doors should be preserved where possible, respecting the shape of the opening, the divisions within it, and the surrounding trim.
- » In addition to providing light, views, and ventilation, windows are an important design element of a heritage building facade. The location, size, shape and style of windows help to establish our impression of the building. Replacement windows should duplicate the originals in style, type and material. Custom wood sashes should be used to provide a near-perfect match for the original. If an exact duplicate is not possible, windows with similar operation and internal divisions should be installed.
- » The size, location or number of openings on the main facades should not be changed, unless the property is being restored to its original appearance. In cases where the interior has been renovated and the ceiling lowered, a bulkhead or valance can finish the ceiling edge against the window. In some cases, an opaque upper pane may also be appropriate.
- » Heritage buildings should be painted in colours matching the original selection. Historic photographs (supplied by the local heritage society), and paint samples/fragments (retrieved from the structure if possible) will provide useful information.

## 4.4 LaHave District

The LaHave District has developed into a more 'box style' commercial area that supports the retail environment of the Historic Downtown. This district is also capable of supporting higher density mixed use development due to its proximity to the town centre, abundance of existing surface parking, and connection to the LaHave River.

### Mid-Rise Apartment Buildings and Condos

#### General Principles:

- » For new residential development in the LaHave District, multi-family development is required. Town home and single unit residential are not permitted.
- » All mid-rise developments must have an active ground floor use that relates to the streetscape. Ground floor residential is not permitted in a multi-unit building.
- » The quality of new residential development must be high.
- » New buildings must demonstrate a definitive architectural style.
- » New buildings must be 'street-related'.
- » New buildings should not exceed 8 stories.

#### Guidelines

- » All buildings greater than four units should be designed by an architect registered to practice in the Province of Nova Scotia.
- » All buildings should be designed to an architectural style that is compatible with the Town's architectural forms (Maritime, Queen Anne, Victorian, Craftsman, Colonial, Second Empire). Each building should have a clearly identifiable style. Windows, doors, roofs, porches, chimneys & other details should reinforce the style.

4.39 Mid Rise Mixed Use



#### Site

- » Locate parking either internally to the building, underground, or in behind. Street-facing surface parking for multi-unit residential is not permitted.
- » Ensure that driveways for vehicular and service access have minimal impact on the streetscape. Service areas should be behind or internal to the building. Driveways should be a maximum width of 6 metres.
- » No net increase in surface run-off is permitted.

#### Height & Massing

- » New apartment or condominium buildings should be no less than 2 storeys and no more than 8 stories.
- » Buildings of 2 storeys should have an architecturally distinct base (first and second storey), and top (roof).
- » Buildings of 3-6 storeys should have an architecturally distinct base of 1 or 2 stories, a distinct middle, and the upper floor shall be incorporated into the roof design (i.e. the roof eave is no higher than the 1.2 metres above the finished floor elevation, with gabled windows allowing an occupy-able space.).
- » Buildings over 3 storeys should incorporate a 2 metre minimum 'step back' from the front facade to minimize the building's mass to pedestrians on the street.
- » Corner buildings should incorporate articulation and forms that emphasize the importance of the corner.

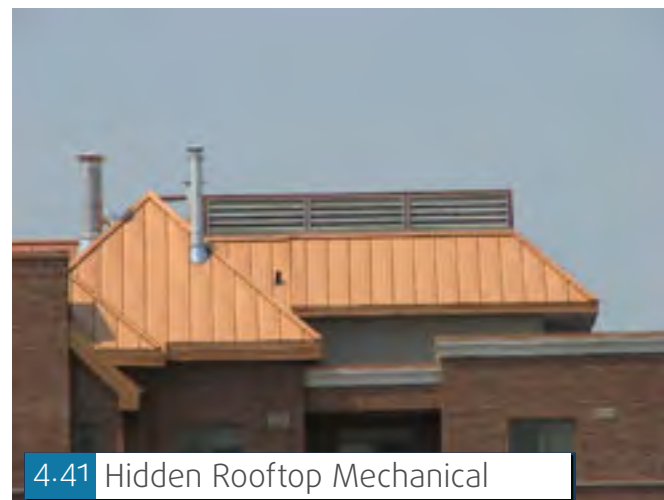
#### Entries

- » Primary entries to multi-unit buildings must front on an adjacent street.
- » All buildings should have a high quality entry plaza no less than 10 square metres as part of the streetscape between the sidewalk and the building entry.
- » At a minimum, the entry plaza should be 1 metre in depth by the 10 metres in length.
- » The entry plaza must provide seating, either by a seat wall or site furnishings. The plaza should be made of pavers, stone, or concrete (or a combination of).
- » The entry should be a highly visible architectural feature incorporating elements such as awnings, roof overhangs, lighting, double doors, glazing and/or distinct architectural materials.

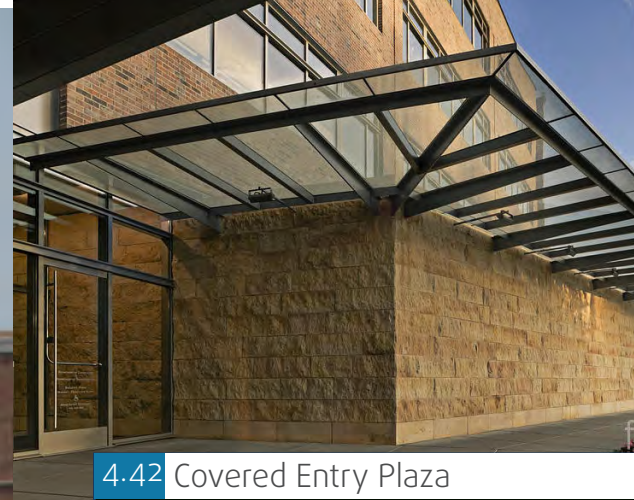
## Details

- » Mechanical or communication appurtenances larger than 0.50 square metres should not be visible from the street.
- » Loading and service areas should not occupy more than 6 metres of building or street frontage, and should be screened from view by enclosure. These areas should be designed as an integral part of the building and made of finished materials compatible with the design of the primary building.
- » Awnings or overhangs are required to cover at least 20% of the ground-floor facade. Overhangs must have at least 2.7 metres of clearance for snow clearing, may not project more than 2/3 of the width of the sidewalk or a maximum of 3 metres, and should be no wider than a single storefront or architectural bay, whichever is narrower.
- » Any patio facing the street must be screened using the prevalent facade material (brick, shingles, steel, glass, etc.) to at least 0.75 metres above the floor elevation.
- » The bottom of any patio, as a visible element of the building, must be afforded the same scrutiny as the siding material on the building. Plain concrete finish or wood decks are not permitted.
- » Buildings greater than 12 metres of frontage length need to be broken down into horizontal compartments with a distinct change in vertical architectural articulation using different facade materials, projections, bays, roof changes, colours, etc.
- » All private balconies will have a retractable laundry line installed as a standard feature.
- » Rooftop mechanical equipment and vents should be incorporated as an integral part of the building design wherever possible. Roof top units and vents should be set back from the roof edge and screened using materials complementary to the building.

4.40 Mid Rise Multi-Family



4.41 Hidden Rooftop Mechanical



4.42 Covered Entry Plaza



4.43 Mid Rise Multi-Family



4.44 Big Box Mixed Use



4.45 Big Box Mixed Use

## Ground Floor Mixed Use

### General Principles

- » All mixed-use buildings in the LaHave District should have an active commercial or institutional ground-floor.
- » Bicycle parking facilities should be provided to encourage alternative modes of transport, particularly for employees and consumers with small purchases.

### Built Form

- » Main entries shall be located along primary streets, with prominent corner entry to buildings located at street intersections.
- » Windows and doors shall occupy no less than 60% of the ground floor facade area for buildings fronting on streets between a height of 1.2 and 2.0 meters above grade. The use of clear glass (at least 88% light transmission) on first floor storefronts is recommended. Reflective or tinted glazing should not be used, however passive shading devices such as "special E" glass technologies, deep overhangs, or external sun shades and trees may be used.
- » Any ground-level retail windows shall be kept visible at night (i.e. unshuttered).
- » If a facade extends along a sidewalk, no more than 40% of its length, or 15 metres, whichever is smaller, may be blank.
- » A wall with a green mural is not considered a blank wall.
- » Doors for retail shops should include a high percentage of glass area. Security grilles along street frontages are discouraged unless integrated within the overall architectural theme.
- » Primary building entrances should directly front onto, and connect to, public sidewalks or open spaces. Secondary entrances may face interior plazas and open spaces, alleys, and surface and parking lots.
- » Overhangs or protected shelters are required over all public entry ways into multi-unit developments.
- » Architectural lighting should wash the entire first storey storefront at night.
- » The location of service area entrances along roads should be discouraged where possible. It is preferable to locate service areas off side streets or service driveways.

## Large Format Commercial

### General Principles

- » New or big box commercial shall be encouraged to develop with integrated mixed use.
- » No net increase in surface run off is permitted.

### Guidelines

- » No additional large format commercial (footprint greater than 1500 square metres) shall be permitted in the LaHave District unless it can be incorporated into a multiple storey development.
- » Minimum height for large format commercial shall be two storeys, minimum of 6 metres in height.
- » The base, middle and top of the building facade should be expressed through the use of materials and detail design.
- » Blank or single material facades that extend the entire length of the building parallel to the public street or the river should not be permitted. Blank walls in other locations, which are visible to the public, should incorporate additional architectural detailing and/or signs, murals, green walls, sculptural, or graphic design
- » Facades of any significant size should be subdivided through a combination of windows and projections and recessions in the building wall to create a consistent rhythm across the facade and establish divisions that express a hierarchy of entrances and identify individual businesses, where applicable.
- » At least 60% glazing should be provided on the at-grade primary building facades and areas that have public activity. Glazing should be actively used to provide storefront windows or merchandise displays. 'Spandrel' or faux glazing should never be used at street level.
- » Main entrances to buildings should be emphasized through canopies, awnings or taller, non-habitable building structures. The volume and height of such structures emphasize the prominence of entrances particularly at a corner location.
- » Colonnades, covered walkways and porticoes are recommended as a means of weather protection and adding articulation to the building elevation. These building projections should be allowed to project beyond the minimum front setback line, but should not extend beyond the front property line.
- » Colonnades, covered walkways, porticoes and other substantial structures should be permanently roofed. Lighting and landscape elements should be incorporated into the design of these structures to promote their use.
- » Pitched or sloped roofs should be considered as alternatives to flat roofs for commercial development, provided that sloped roofs respect the context and rooflines of adjacent buildings.
- » Rooftop mechanical equipment should be integrated with the building design and rooftop units and vents should be screened using materials complementary to the building. Where appropriate, parapets should be used to screen rooftop mechanical units.



4.46 Big Box Mixed Use

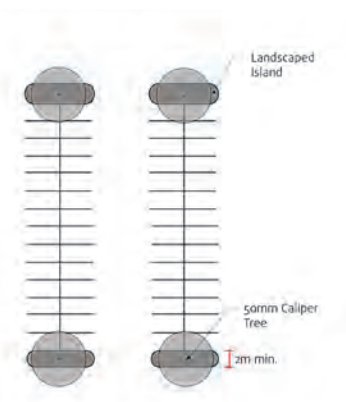


4.47 Large Format Commercial

## Surface Parking

### General Principles

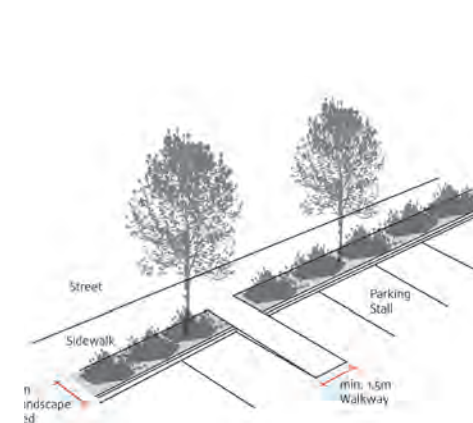
- » On-street parking should be provided where possible to compliment existing surface parking.
- » Off-Street Surface Parking should be configured and designed to reduce its visual dominance of from public streets.
- » Off-Street Surface Parking should incorporate high quality walkways and alleyways to connect parking lots to public streets and safely separate pedestrian and vehicle movements.
- » Off-Street Surface Parking should be designed to provide a strong visual quality through the use of high quality landscaping, lighting, and pavement materials.
- » Off-street parking should maximize capacity of rear lots by encouraging neighbouring land owners to work together to restructure and reorganize parking layout. The Town should continue to work with land owners to help facilitate this process.
- » Off-Street Surface Parking should be designed according to environmental sustainability principles, including the minimization of surface water runoff and 'heat island' effects.
- » No net increase in run-off is permitted.



4.48 Parking Islands



4.49 Infiltration



4.50 Parking Buffer



4.51 Permeable Paving

### Guidelines

- » Entries to rear-lot parking will be permitted (no more than 6m wide) and single loaded parking on these entries will be permitted.
- » All off street parking shall be separated by no less than 2m of landscaping and/or sidewalk between it and the street.
- » The width of the entry road and parking stall shall not exceed the width of the building facade fronting on the street
- » For every parking stall which fronts on the street, the developer will be required to construct a 2m wide landscape bed between the stall and the sidewalk and install at least one 50 mm caliper street tree for every 9 metres of linear frontage. The plant species and condition are to be approved by the Town.
- » No parking should be located between the street and the building
- » A landscaped island should be located at each end of every parking aisle. Landscaped islands should be a minimum width of 2m wide and include one 50 mm caliper sized tree per parking row.
- » Limit the length of parking rows to a maximum of 32 contiguous spaces per row. Longer spans should be broken up with landscaped islands (rain gardens). All catch basins should be located at or near these islands.
- » Landscaped islands to be designed as stormwater catchment areas. Sub-base and base material are to be high performance granular material ideal for infiltration and interception of groundwater. When necessary, french drains or perforated-bottom catch basins will be installed, to further facilitate infiltration.
- » Parking Islands will be curb-less, or have frequent curb cuts, and the surrounding area will be graded to drain towards the islands. Concrete curb stops should be used, if required.
- » Lighter asphalt is preferred over darker asphalt to reduce urban heat island effect. Asphalt should include a high percentage of white aggregate. Concrete is preferred over asphalt where practical.



4.52 Parking Section

## Bicycle Parking

### Guidelines:

The minimum and maximum number of required bicycle parking spaces shall be as follows:

Use	Parking Requirement	
Multiple Unit Dwelling (four or more dwelling units)	0.5 spaces per dwelling unit.	80% class A, 20% class B
Hotels or Inns	1 space / 30 rooms.	80% class A, 20% class B
Commercial / Office	1 space / 500 sq.m GFA	80% class A, 20% class B
Entertainment	1 space / 25 seats	20% class A, 80% class B
Institutional Uses	1 space / 250 sq.m GFA	20% class A, 80% class B
Parking Garages	1 space / 20 parking stalls	Maximum of 50 bike spaces.
Any other use	1 space / 500 sq.m GFA	50% class A, 50% class B

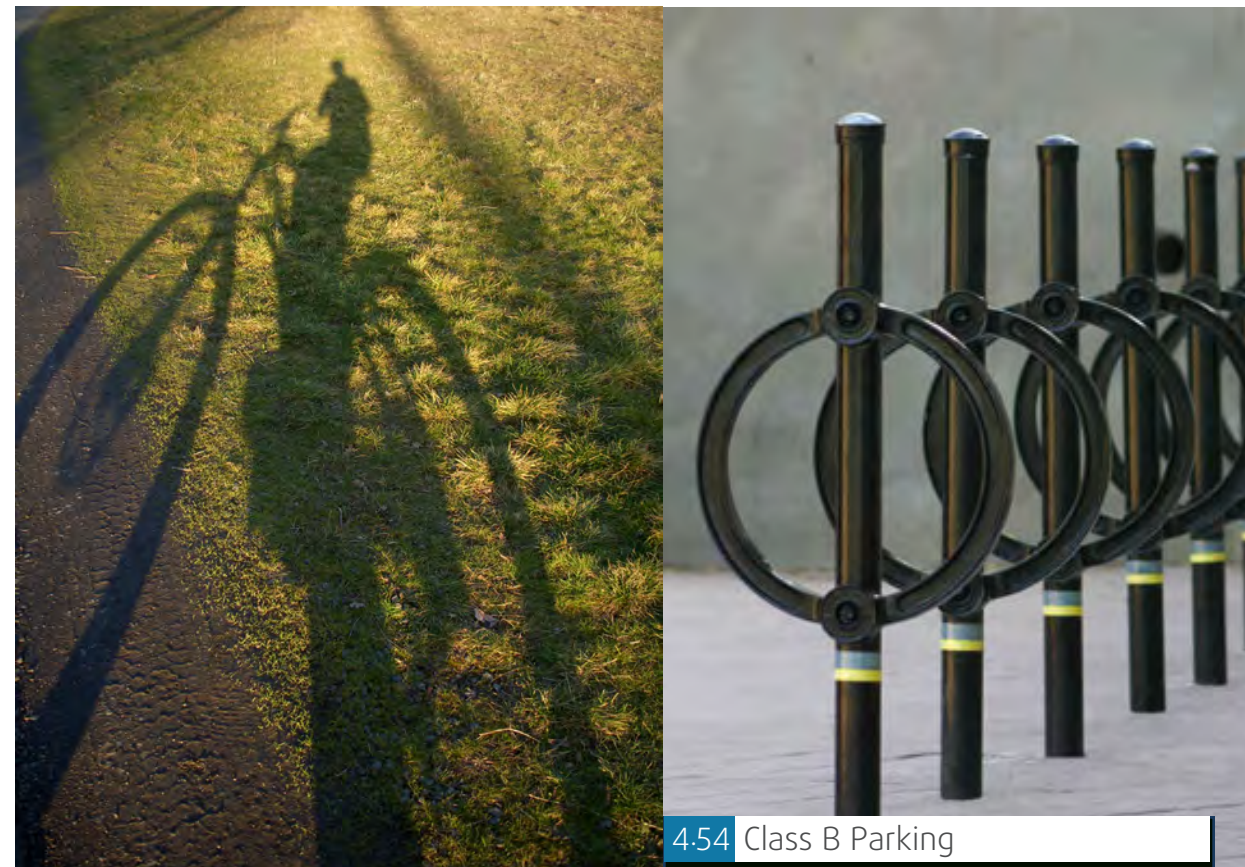
4.53 Parking Requirements

### Class A Bicycle Parking means:

- » Covered parking spaces protected from precipitation within 50 meters of the building's primary entrance.
- » Parking spaces shall be highly visible, at ground level or monitored by constant security cameras. Parking areas to be well lit.
- » Directional signage shall be provided.
- » Parking spaces shall have a minimum overhead clearance of 2 metres.
- » Parking stalls shall be no less than 0.6 meters long and 1.2 meters high, and have an associated aisle of 1.5m in width. Door openings shall have an opening of 0.6 meters.

### Class B Bicycle Parking means:

- » Parking is located within 200m of the primary building entrance.
- » Parking spaces shall be no less than 0.6 meters long and 1.2 meters high, and have an associated aisle of 1.5m in width.
- » Bicycles should be supported at all times by a minimum of two attachment points.



4.54 Class B Parking



4.55 Class A Parking

## Signage

### General Principles:

- » Signage should not be an afterthought in the building design but instead should be integrated into the design as a necessary element.
- » All signs should convey their message clearly and legibly, shall be vandal and weather resistant, and if illuminated, shall not be overly bright for their surroundings.

### Sign Elements:

- » A sign board for commercial signage or awnings is required above at the top of the first storey and must be integrated into the architecture of the building.
- » Signs are permitted anywhere between the first and second floors. Signs on the top storey are permitted provided they are no greater than 0.5 square metres times the number of storeys (i.e. a 6 storey building can have 3.0 square metre sign).
- » Projecting signs can be no larger than 2 square metres. Projecting signs must have a clearance of 2.7 metres above grade. Preferred materials for suspended or projecting signs include wood, metal, or a high-quality composite material.
- » Fixed wall signs may not exceed
  - » Awning signs are encouraged along the street. Awning sign copy should be limited to the business name, or logo. The sign size should be in proportion with the awning. Awning graphics must complement the awning colour.
  - » Spot lights, gooseneck light fixtures and other decorative light fixtures should be utilized for illuminating awning signs.
  - » Buildings with businesses that do not have ground floor usable space may have one 2.5 square metre sign, such as a directory sign, to be utilized by all above ground floor businesses so long as

those businesses do not also occupy ground floor usable space. Sign to be placed adjacent to the primary entrance for the above ground floor businesses.

- » New signs proposed for existing buildings should provide a compatible appearance with the building signage of other tenants. When there are multiple signs on a single building, there should be an effort to bring in a unifying element (such as size or material), even where no sign program exists.
- » Signage which contains business slogans or advertising is not permitted. However, signs may include information describing the products sold or services provided.
- » Repetitious signage information on the same building frontage shall be avoided regardless of the sign area square footage allowed.
- » Temporary hand painted signs, vinyl signs, or vinyl lettering is not permitted.
- » Billboards, free-standing pole signs, monument signs, marquees, any kind of animation, roof, and signs painted on the exterior walls of buildings are prohibited. No flashing, traveling, animated, or intermittent lighting shall be on the exterior of any building whether such lighting is of temporary or long-term duration.
- » Modern low-energy LED lighting is required in all signs.
- » No business in the LaHave District shall exceed one wall sign and one projecting sign per exposed facade.
- » Wall mounted signs shall not exceed 20% of the building's facade surface area. No singular sign may exceed 28 square metres in size.

4.56 Signage Examples



4.57 Signage Examples



## Architectural Materials

The quality of the architecture will set the standard for the character and sense of place that evolves in the downtown. New developments should integrate with existing structures through consistent design and material quality, while encouraging contemporary and individualized architecture.

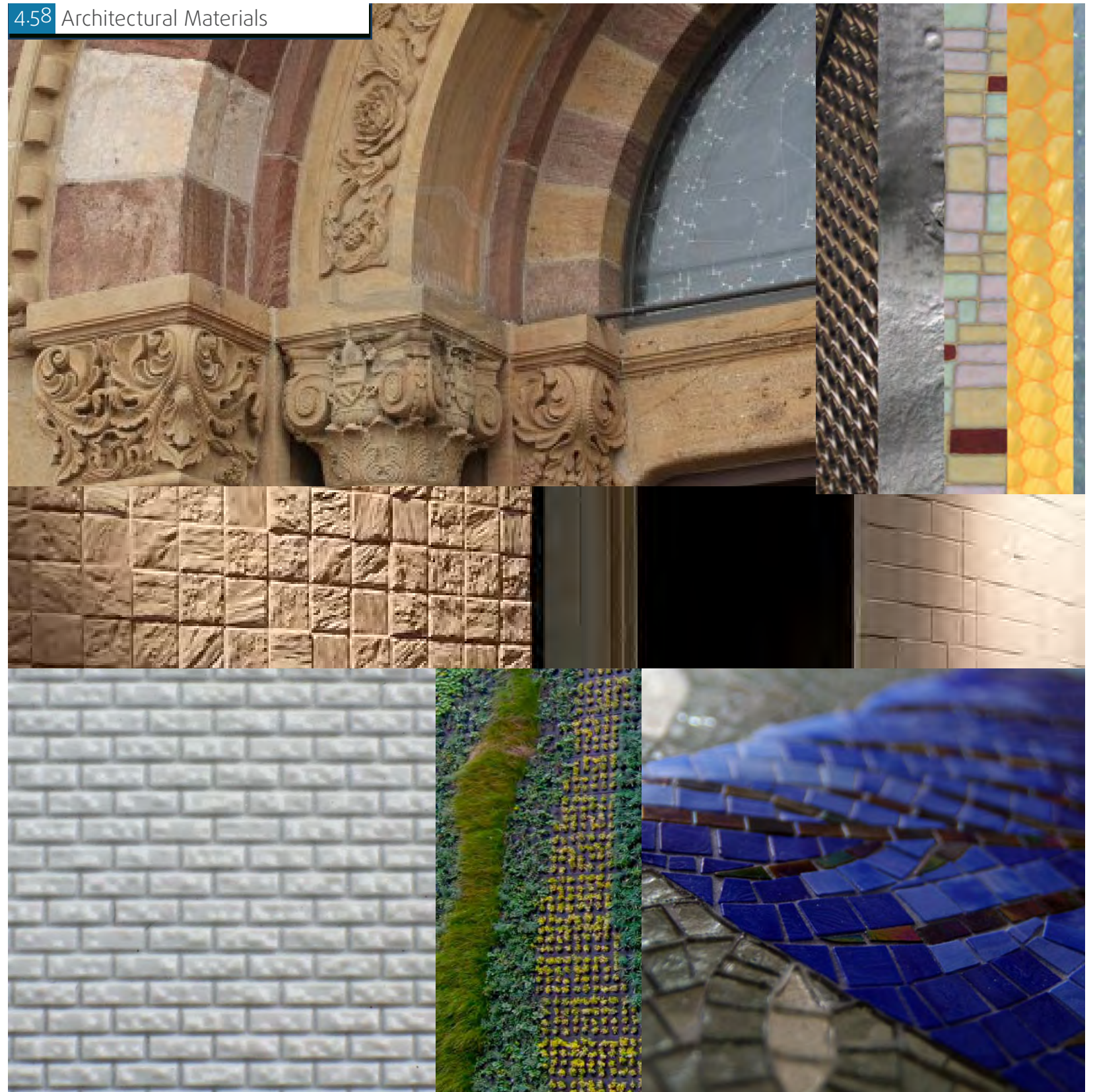
### General Principles:

- » These guidelines appeal to all new development or adaptive reuse projects in the Downtown.
- » Building materials for the LaHave District should be high quality.
- » Sustainable building practices are encouraged in the LaHave District, including the adoption of LEED-based principles or specific programs.
- » Materials used on the facade of any structure should be carried around the site, if the streetwall is not continuous.

### Guidelines

- » The use of traditional, natural, high quality, durable materials which enhance the building's aesthetics and convey a sense of permanence shall be required. Green building materials, or materials produced in a sustainable manner are preferred.
- » Wood, cementitious board, red clay brick, high quality metal, glass and/or natural stone is encouraged as the primary exterior finish material. Aluminum siding, vinyl siding and stucco are not permitted. Cladding materials may include brick, stone, metal, wood, glass, in-situ concrete, and pre-cast concrete (excluding split face concrete, or concrete block).
- » In general, the appearance of building materials should be true to their nature and should not mimic other materials. Any product that mimics a more expensive material is not permitted (e.g. stone tile, brick tile, etc). Vinyl siding, plastic, plywood, concrete block, mirrored glass and metal siding utilizing exposed fasteners are not permitted.
- » In general, heavier materials (like stone or metal) shall be used at the building's base, and lightest materials (glass or wood) shall be used at the building top.

4.58 Architectural Materials



## 4.5 Planning Policy Amendments

The following policy review is intended to suggest broad changes or additions/deletions to existing municipal policy in order to cement the principles of the Downtown plan into the Municipal Planning Strategy (MPS) and Land Use Bylaw (LUB). The MPS contains the general policy statements with regard to municipal intent (vision, principles, goals, objectives), while the LUB contains the specific policy 'rules' for managing land uses in the Town. The MPS and LUB are typically updated on a 5-10 year schedule as a full public process which is required by the Municipal Government Act (MGA). The existing MPS and LUB are presently under review, as part of the Planning Review process.

The following policy suggestions should be considered as part of that review.

### Municipal Planning Strategy (MPS)

The existing MPS is structured into broad land use groupings (residential, commercial, institutional, industrial), social and cultural considerations, and infrastructure. A detailed policy review is beyond the scope of this project. The following general recommendations are suggested for future revisions:

- » Include a policy chapter on sustainability. Many other communities are integrating principles of sustainability into their MPS, even if they have adopted an Integrated Community Sustainability Plan (ICSP). The Bridgewater downtown and waterfront plan emphasizes a compact, walkable, mixed use core, with particular emphasis on mixed-use residential development so that more people can work, live, and play within a self contained area. The sustainability chapter should also address AT-planning, bicycle friendly Downtown, transit oriented development, urban forests, open space, etc.
- » Adopt a policy on restricting banks, business, and professional offices outside of the historic downtown. This single policy has prevented the out-migration of the service industry from the Downtown in many other Towns.
- » Add a new land use designation, Downtown Mixed Use Area, which shall replace the Central Commercial Zone. Within the Downtown Mixed Use Area, the following land uses are appropriate:
  - » Commercial
  - » Residential
  - » Mixed-use
  - » Parks and Open Space

- » Recreational Facilities
- » Cultural Facilities
- » Institutional Uses and Public Buildings
- » Other uses deemed to be compatible and desirable for the Downtown area.
- » Within the Downtown Mixed Zone, the requirements of the LaHave River Development Agreement Area shall be apply. Any development that is not as of right shall be subject to a development agreement process.
- » Section 12: Architectural Heritage. Expand the architectural control area designation to the entire Downtown Mixed Use Area. All development in this zone is subject to a design review process.

### Land Use Bylaw

The Bridgewater Downtown and Waterfront master plan seeks to create a vibrant Downtown core with a variety of shops, services and housing options. Attracting businesses and adding more residential development will animate public spaces and increase traffic to the downtown, encouraging spending and vitality. In order to achieve the vision set forth by this project the following general recommendations have been suggested with respect to the existing Land Use Bylaw, to be addressed and integrating into the ongoing Planning Review.

The following general recommendations are suggested for future revisions:

- » A new zone shall be created, Downtown Heritage Zone (DH).
- » A new zone shall be created, LaHave District Zone (LD).
- » These two zones shall comprise the Downtown Mixed Use Area, as defined by the MPS.

### Downtown Heritage (DH) Zone

The Downtown Heritage Zone shall follow the provisions of the General Commercial Zone (C1), with the following amendments:

- » Under permitted developments, remove: automobile sales establishments, automobile service stations, automobile washing establishments, warehouses, equipment rental depots, garden and nursery sales and supplies, motor vehicle repair shops, and private clubs.
- » Add a new section Prohibited Developments. Drive-through restaurants or banks, self storage facilities, should not be permitted in the Downtown Heritage Zone
- » Under Developments Permitted Subject to Other Requirements of this Bylaw add: Conversion of existing buildings to a maximum of 6 units, in accordance with the requirements of Subsection 5.3.
- » Under Zone Standards:
  - » Increase maximum height of building to 6 storeys or 20 metres, whichever is shorter.

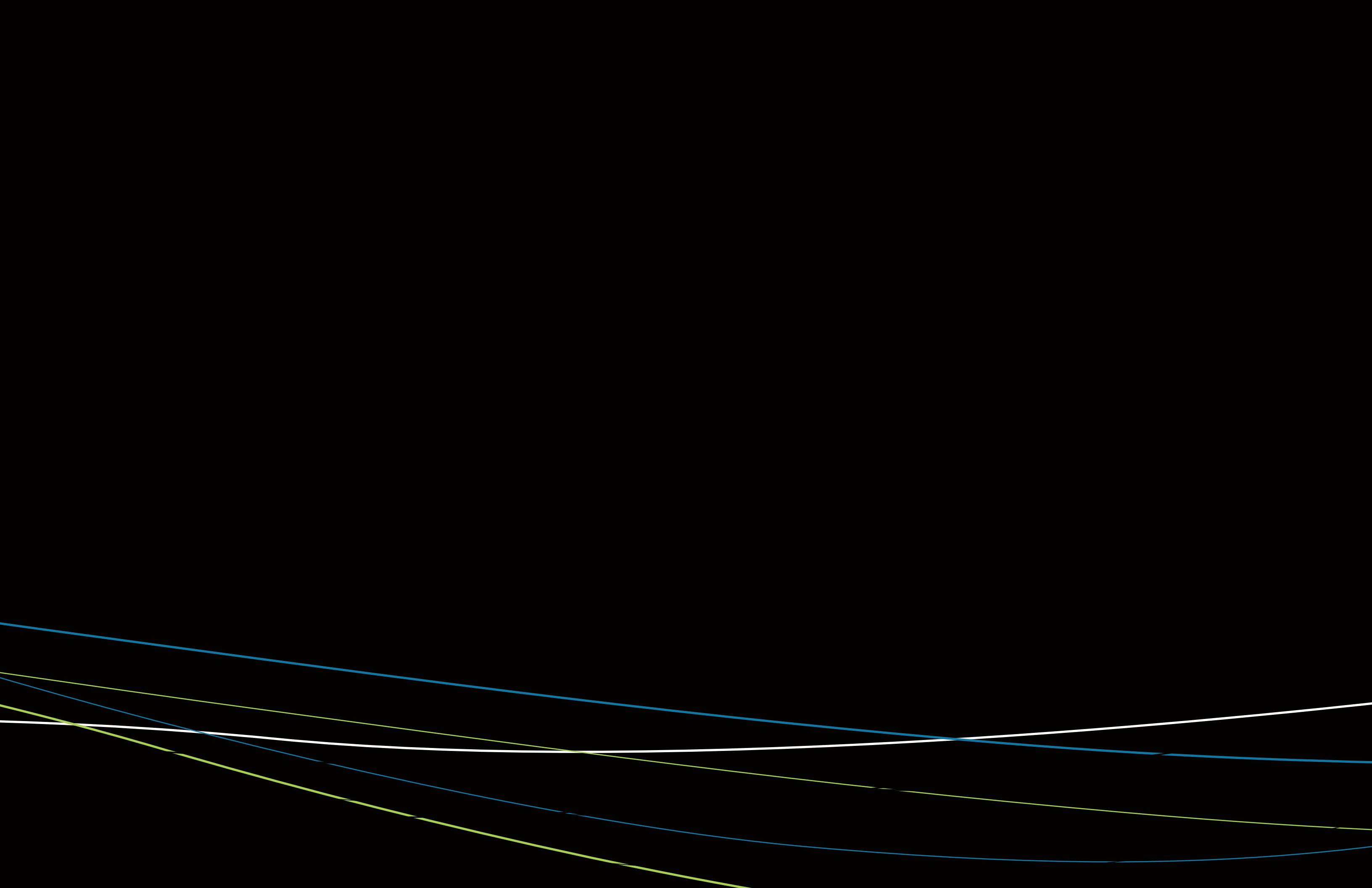
### LaHave District (LD) Zone

The LaHave District Zone shall follow the provisions of the Group Commercial Zone (C1), with the following amendments:

- » Under permitted developments, remove: automobile sales establishments, warehouses & distribution centres, funeral homes, boarding and rooming houses, private clubs, and self storage facilities.
- » Under Zone Standards:
  - » Increase maximum height of building to 8 storeys or 27 metres, whichever is shorter.

4.59 Proposed planning districts





# MOVING FORWARD

*ECONOMIC DEVELOPMENT SPECIALISTS  
JUDGE A COMMUNITY'S VITALITY BY  
THE QUALITY AND CHARACTER OF ITS  
CENTRAL BUSINESS DISTRICT  
- DECATUR MAYOR ERIK C. BRECHNITZ*

The following section outlines a series of actions and recommendations for the implementation of the Bridgewater Downtown and Waterfront Master Plan. The plan itself is consistent with both the objectives of the project as described by the steering committee and the vision articulated by the public throughout the public engagement process.

## 5.1 Recommendations

### Adopt the Plan

The first step towards implementation is for Bridgewater Town Council to adopt this master plan. This will empower staff and Council to allocate the necessary resources and to begin to develop and expand upon relationships that will be needed to see the projects move forward. Even in the event that there are recommendation within the plan that Council does not agree with, the adoption of the plan will facilitate staff and Council addressing those details as the plan moves forward.

### Integrate Planning Policy Recommendations

One of the primary principles of downtown revitalization is the creation of an environment that encourages and supports desirable development (Make the Right Thing Easy, Step Four of the Brookings Institute 12 Step Program). Much of the creation of a supportive downtown environment relates to the existing planning and development policies. As one of the early steps towards implementation of the plan, the recommended policy amendments and revisions should be integrated into the Land Use Bylaw (LUB) and Municipal Planning Strategy (MPS). For Bridgewater, this step will be facilitated by the ongoing Planning Review process.

Key recommendations for Planning Policy Amendments.

- » The creation of a new land use, Downtown Mixed Use Area, for the Municipal Planning Strategy.
- » The creation of two new zones, the Downtown Heritage (DH) zone, and the LaHave District (LD) zone.
- » Development in the Downtown Mixed Use Area shall supersede the LaHave River Development Agreement Area, and shall proceed as of right, or by site plan approval.
- » The architectural control area shall be extended to encompass the entire Downtown Heritage Zone. Development shall be subject to a design review.

## Initiate Strategic Partnerships & Funding Opportunities

Many of the challenges associated with downtown redevelopment relate to the need to coordinate multiple interests, landowners, existing land uses, and long term visions, and address how these diverse perspectives will be rationalized, phased, and financed. In moving the plan forward, it is imperative that the Town take a leadership role in bringing together diverse stakeholders. This does not mean that the Town needs to accept the majority of the capital costs, but rather foster the development of strategic partnerships. There are a number of steps that need to be taken, to encourage and support the implementation of this plan, which could include:

- » Land Assembly: One of the current challenges to downtown development is the lack of available land. In the short to medium term, the Town should begin the process of assembling strategic parcels, with the long term intent to create a consolidated, more desirable development site. There will be acquisition costs in the short term, but in the long term the Town could ultimately turn a profit on the real estate transaction, with the added benefit of creating more realistic and attractive development sites for the private sector. The intent is for the Town to own the land only for a short time, and resell it to the market when combined with other adjacent properties.
- » Land Acquisition: There may be properties that have significant value as public lands, either for open space, public facility or other beneficial use. In this case, the Town should acquire the land with the intent to hold the property as a municipal asset.
- » Public-Private Partnerships (P3). The Town should explore the possibility of partnering with the private sector for the development of key facilities or sites. This allows the Town to share the costs / risks / benefits of development with experienced partners, and further encourages redevelopment of high profile sites in the downtown.
- » Landowner Agreement. There are areas in the downtown that require update or renewal, but are owned by multiple landowners (the parking lot behind Town Hall is one example). The Town should facilitate the development of landowner agreements for the necessary renewal of these spaces. Again, the capital costs to the Town are low, and the potential for public benefit is high. A landowner agreement provides a mechanism for a cooperative project to proceed with clear financial and legal division of responsibility.
- » Tax Increment Financing: Although not currently permitted under the MGA, Tax Increment Financing (TIF) could allow the Town to provide the initial financing to pay for infrastructure and public realm renewal projects. The Town recoups their costs in the future from increased property tax revenue generated by the renewal, and subsequent private investment. The Town should work with other municipalities to advocate for this initiative.

## Undertake Supporting Technical Studies

The Bridgewater Downtown and Waterfront Master Plan outlines a twenty year vision for the future of the Town's central area, and provides a series of physical projects, policy amendments, and programming recommendations designed to achieve that vision. While many of the projects and initiatives are straightforward and 'shelf' ready, some of the projects will require supporting detailed studies to facilitate their implementation. These studies should include detailed construction drawings and associated costing and phasing plans, to allow for realistic budgeting by the Town.

### Engineering Studies:

- » Detailed design for the King Street Reconstruction
- » Detailed design of North Street & King Street roundabouts
- » Shoreline Stability / Flood Mitigation
- » Municipal Transit Feasibility Study

### Planning & Public Realm Studies:

- » King Street Urban Design Study
- » Public Art Strategy
- » Schematic Signage Design
- » Design of Old Bridge Landing Park
- » Green Wall Design Guidelines

### Province - Lead Studies:

- » Detailed Design for the New Bridge Crossing

## Other Initiatives

### Pursue Alternative Funding Sources

A variety of funding mechanisms are in place to assist the Town with the capital costs for some of the proposed recommendations. Alternative funding will be important during the early phases of implementation, as potential tax increases will not yet have been captured, and public investment will be essential to the plan's success.

- » Federation of Canadian Municipalities Green Municipal Funds
- » Federal Gas Tax Money
- » Private Sector 'Green' Infrastructure Grants
- » ACOA Economic Development Funding



## 5.2 Plan Prioritization

This report describes both a long-term 20-year vision, and achievable short term projects for the evolution of downtown Bridgewater. The plan components and projects are consistent with the vision outlined during this process, and will work together towards fulfilling the Community's goals.

When developing a prioritization and phasing Strategy, initiatives with a high profile and ease of implementation should be given priority, especially where cost is not prohibitive. Larger and more complex projects will often require time, funding, or further study to address the details required for implementation.

Setting priorities for implementation should be based on the following criteria:

- » Immediate economic impact;
- » Best probable funding opportunity;
- » Timeline for possible environmental, infrastructure, or land acquisition issues;
- » Potential for greatest positive impact;
- » Ability to link open spaces or other sites;
- » Status of construction readiness;
- » Opportunity for partnerships with the private sector;
- » Coordination with ongoing or future municipal projects;
- » Logical design and construction sequence.

Forecasting a year-by-year phasing Strategy is difficult without a strong sense of annual budgets. Priorities set by the Steering Committee can be influenced by the opportunities that arise from unforeseen funding sources, new developments, and private sector initiatives.

	Category	Plan Project or Task	Sub Project	Intent	Primary Champion	Aligned Organizations or Partnerships to Foster	Time Frame	Measure of Success	
1	Physical Projects	King Street Reconstruction	Old Bridge Street to Dufferin Street	Major circulation improvements will facilitate pedestrian and vehicular traffic to and through the downtown. Streetscaping will enhance the pedestrian environment, making it safer and more aesthetically appealing. New vehicular routes will add improved efficiency and traffic capacity in the downtown.	The Town	The Province,	Phase One	Project is completed.	
2			Victoria Road to Old Bridge Street		The Town	The Province	Phase Two	Project is completed.	
3			King & Victoria Roundabout		The Town	The Province	Phase Three	Project is completed.	
4			South Parkade Removal		The Town	The Province	Phase One	Project is completed.	
5		New Bridge	Old Bridge Pedestrian Upgrades		The Province	The Town	Phase Three	Project is completed.	
6			Construction of a New Bridge		The Province	The Town	Phase Three	Project is completed.	
7			Reconfiguration of the Old Bridge		The Town	The Province	Phase Three	Project is completed.	
8		LaHave Street Upgrades	Victoria Road to Old Bridge Street		The Town	The Province	Phase Two	Project is completed.	
9			Old Bridge Street to Empire Cinemas		The Town	The Province	Phase Two	Project is completed.	
10		North Street Roundabout	North Street Roundabout		The Town	The Province	Phase Two	Project is completed.	
11		Designation of Truck Routes			The Town	The Province	Phase One	Project is completed.	
12		Recreation Waterfront	North Basin Improvements			The Town		Phase Two	Project is completed.
14					Riverside Park	The Town		Phase Two	Project is completed.
15					Waterfront Boardwalk	The Town		Phase Two	Project is completed.
16					South Park Marina	The Town		Phase Two	Project is completed.
17		Programming	Downtown Destinations		New Cultural Centre	Support the development of downtown destinations that will encourage traffic and activity in the Town centre.	Private Group		As market conditions allow
18			Conversion of the Library		Private Sector	The Town	As market conditions allow	A new use is established and thrives in this building.	
19	Green Mural Program			A unique approach to beautifying the downtown.	The Town / Business Improvement District	The Town	Program establishment: Phase One Implementation: As private interests allow	One mural / year for the first five years after establishment of the program	
20	Signage & Wayfinding		Signage Master Plan		A strategy to increase the visibility and legibility of the Town, with a primary focus on wayfinding to and through the downtown core.	The Town		Phase One	Completion of this study.
21				Gateway Signs		The Town	Phase One and Two, as budgets and requirements allow	Fabrication and installation of new signage family for the Town.	
22				Wayfinding Signs		The Town			
23				Street Blade Replacements		The Town			
24				Banner Program		The Town			
25				Interpretive Signage		The Town			
26				Information Kiosks		The Town			
27	Land Strategy		Land Acquisition		Bridgewater Development Association	The Town			As market conditions allow
28				Land Assembly	Bridgewater Development Association	The Town	As market conditions allow		
29				Residential Development	Bridgewater Development Association	The Town	As market conditions allow		
30		Mall Mixed Use Site		Zenda Developments		As market conditions allow			
31	Downtown wi-fi		A utilitarian attraction for the downtown.	The Town	The Business Improvement District	Phase One	Project is completed.		
32	Business Improvement District		Coordinate, educate, and facilitate cooperation and collaboration between downtown merchants.	Bridgewater Development Association & Town Centre	The Town	Phase One	A new BID is formed.		
33	Civic Events	Event Coordinator	Programming in the downtown increases traffic and vitality to the area.	The Town	The Business Improvement District	Phase One	Either a dedicated event & partnership coordinator is hired, or existing staff resources are allocated to this responsibility		
34		Partnership Development Coordinator		The Town		Phase One			
35	Policy	Planning Policy Amendments		Creates and supports desirable downtown development.	The Town		Phase One	New amendments are approved and adopted.	

5.1 Implementation Matrix

## Phasing Timeline

- » Phase One: 0-5 years
- » Phase Two: 5-10 years
- » Phase Three: 10+ years

## Phase One Recommended Projects

Phase one projects should be the highly visible, highly requested projects with relatively easy implementation. Starting with visible projects will help to instill confidence with the public that the plan is moving forward, and will build excitement and momentum. The implementation of a highly visible project will also provide the Steering Committee and the Town with a marketing tool to help raise private or external funding for subsequent projects. For Bridgewater, the opportunity to tie a major project into existing gas tax funding and link to planned King Street servicing upgrades is an opportunity that should not be missed.

### Key Public Projects for Phase One:

- » Reconstruction of King Street (Old Bridge Street to Dufferin Street)
- » Removal of the South parkade
- » Designation of formal truck routes
- » Signage & Wayfinding Master Plan
- » Downtown wi-fi
- » Establishment of a business improvement district (BID)
- » Establishment of a downtown events coordinator (shared or dedicated)
- » Establishment of a partnership coordinator (shared or dedicated)
- » Interim modification of the Old Bridge
- » Integration of recommended planning policy amendments into the Planning Review.
- » Adoption of a land assembly policy / Initiation of land assembly process

## Phase Two Recommended Projects

Phase Two projects will begin to really move the entire project vision forward. Projects left to phase two may have some funding or technical challenges, or simply follow logical construction sequencing and need to follow planning phase one projects.

### Key Public Projects for Phase Two:

- » Reconstruction of King Street (Victoria Road to Old Bridge Street)
- » Upgrade of the LaHave Street streetscape
- » Construction of the North Street roundabout
- » Riverside Park improvements
- » Construction of sections of the waterfront boardwalk
- » Construction of the south park marina

## Phase Three Recommended Projects

Phase Three projects may have to be revisited or re-prioritized, based on the ongoing review of the implementation process. Projects left to phase Three are those with significant engineering or implementation challenges, high capital costs, or other impediments to development.

### Key Public Projects for Phase Three:

- » Construction of the King Street & Victoria Street roundabout
- » Construction of a new bridge
- » Conversion of the old bridge to a pedestrian / active transportation connection



## 5.3 Economic Impact Assessment

### King Street Redevelopment Project Economics: Qualitative Assessment

Of course, the goal of any proposed development concept includes tangible benefits. For this project, this includes the expansion of community and social values through the creation of greenspace in an urban core, the renewal of a historic and unique waterfront area, and a focus on the development of a niche service area that plans to offer a range of cultural, culinary, and experiential-based services and goods. These area improvements will enhance the quality of life for residents, as well as offer non-residents to the area a range of memorable services and experiences.

In addition to these broad objectives, the eventual realization of the multi-year project will also support the region by fulfilling specific economic development objectives. The attraction of local and regional residents, as well as non-residents to spend in the area by providing them with a cluster of services will encourage people to stay in the area longer and by extension, increase the probability that they will spend their money in the downtown.

For the Town and the stakeholders who have wisely focused on the historic downtown as one of the region's important assets that needs to be developed, a quantifiable benefit will be the increase in the tax base. As the various phases of the project are implemented, King Street will be renewed, a critical mass will build, activity will increase, commercial operations will achieve greater success, property values will rise, and accordingly, the tax assessment will rise. This will lead to increased revenues for the Town that can, in turn, be reinvested in support of the community.

Along the pathway toward this, the Town will experience economic impacts through a number of specific avenues.

- » **Economic Impacts of Construction.** The economic benefits from the “one time only” construction investment arise because of spending on goods and services in relation to the development itself. This activity spans from the site preparation through to the commissioning of the proposed facilities - the economic impact from the commercial construction spending associated with the plan.
- » This is a ‘one time only’ injection because, although it may occur over a period of several years, the spending ends when the facilities are completed (i.e. the Construction Phase). This includes the \$10 million in total direct spending over 10 years on the proposed new bridge crossing, development of the land parcel adjacent to the new park, various streetscape improvements, various furnishings walkways, paths, signage, public art and other features of the development, the adaptation of the old bridge and any work associated with its conversion to a pedestrian only pathway, development of various waterfront recreation facilities such as launch sites and facilities for visitors and residents (e.g., rentals for canoes, kayaks, paddle boards, and other goods and services, the development of marina dockage at the new park, as well as mixed used development and private investment and development spurred on by the activity). Effects will include direct and spin-off (indirect and induced effects – or the spending of household income) impacts over the entire construction period.
- » **Fiscal impacts to the Town.** As noted, the fiscal impacts to the Town are triggered by the activity and the resulting increase in property values. The development and its effect on local area business will increase the Town's tax base and, therefore, property tax revenues. In this case, these are difficult to define because the nature of the project is largely public infrastructure related and more analysis would be necessary to measure the financial effect of this on adjacent property.
- » **Resident and Visitor Spending Impacts.** The impact of resident spending is leveraged through the effect of the site to help the Town retain more of the local spending that may have otherwise gone to other regions of the province or, indeed, out of province. The ability of the site to entice residents to spend, for example, their entertainment dollars in the community will improve the opportunity for local accumulation of wealth and reduce leakage of dollars outside the community. Likewise, the impact of the development to attract and retain non-residents from other parts of Nova Scotia and from outside the province will be important.
- » **Commercial Operations Impacts.** As businesses take up tenancy in the downtown or LaHave district and /or expand, they will need to spend on locally based goods and services to support their operations. This includes directly hired staff, as well as contract suppliers of goods and services. These are the impacts associated with the direct operations of those enterprises and organizations that take up tenancy within the development area.

Given the conceptual nature of the project, it is only possible to provide an estimate of the economic impact associated with the construction phases. This is mainly because the infrastructure needs are specified in the plan, and although the impacts from the construction phase will be important, the more important and interesting impacts – indeed, the reason

for the development in the first – are the impacts on the tax base, the prosperity of the businesses that benefit from the development, the impact of retaining more local spending by providing more locally-based niche opportunities for spending on goods and services, and the capacity of the development to attract new spending from outside the community.

More qualitative is the potential of the development to play a role in attracting newcomers and/or the retention of current residents. In a world where people can literally work anywhere and live anywhere at the same time, the capacity of a community to offer locally based niche goods and service opportunities are becoming increasingly important determinants of the community's ability to attract and retain community members, particularly those whose work/life balance considerations align with what Bridgewater can become.

## 5.4 Measuring Success

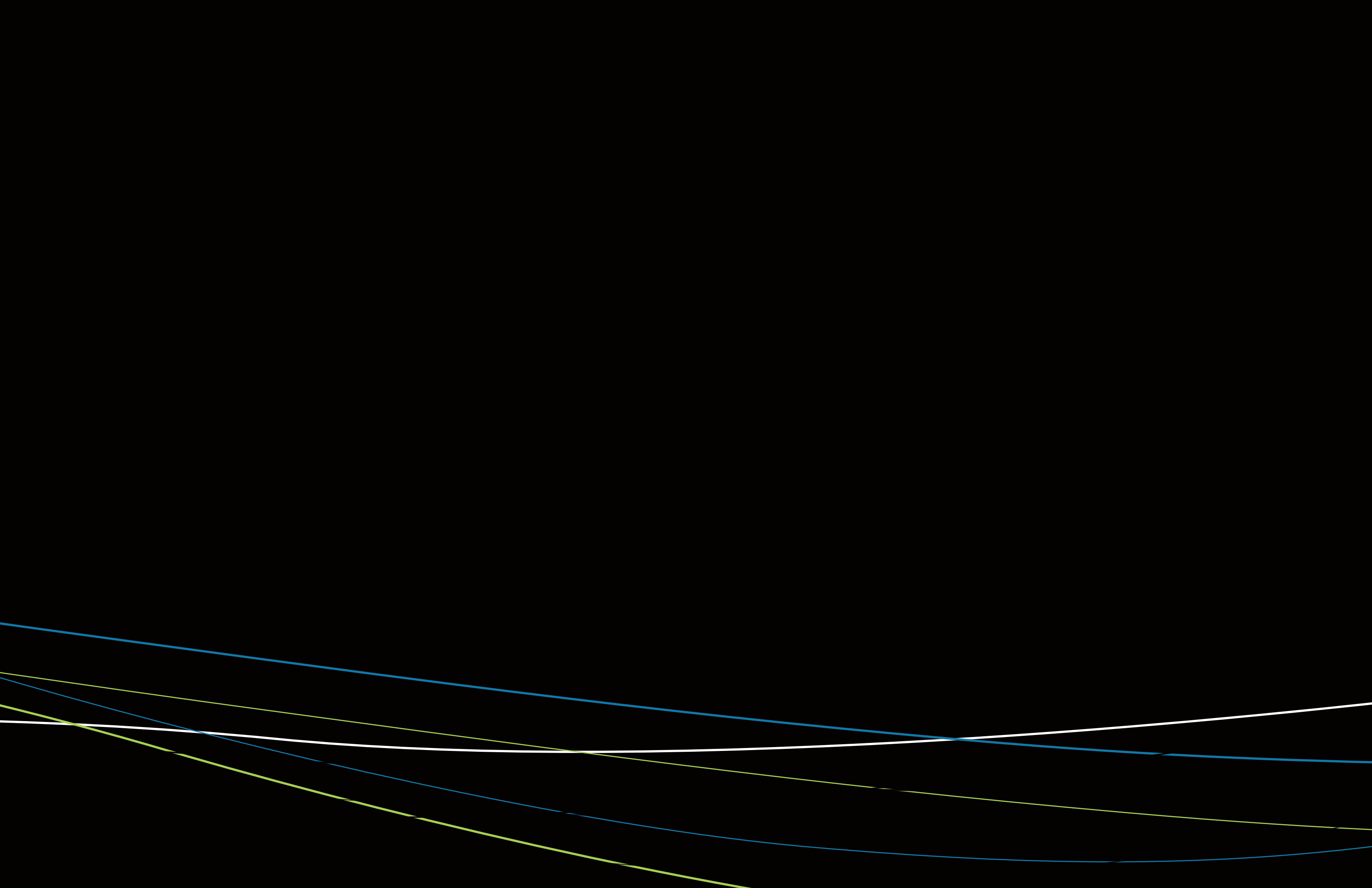
Unlike more conventional planning exercises, which may have clear indicators of successful implementation, downtown revitalization often focuses on more abstract or complex solutions that may be harder to quantify. Therefore, it makes it harder for the Town Council and Staff to track the success of the plan. There are several key categories for measurement and evaluation of community change; factors developed by other communities, which are relevant to Bridgewater at the onset of this planning process. These indicators can be used as metrics to track the progress of realization for the various plan components.

- » Economic indicators: Indicators such as the number and variety of businesses, the number and variety of housing types, and the quality and quantity of office space are all factors relating to the health of the community. Residents and visitors should have a range of restaurants, shopping, entertainment, and lodging types to choose from.
- » Vacancy Rates: The attraction of new businesses to the downtown area will increase revenues, and give visitors and residents options for shopping and services. The more competitive the commercial space is along King Street, the more revenue can be drawn from that space. Successful businesses want to locate in highly visible, high volume venues. Low vacancy rates means that space is in demand, and the overall vibrancy and attractiveness of the downtown increases.
- » Perception: Successful gateways have a variety of activities for a variety of users at a variety of times. Lighting, signage and wayfinding, and people all contribute to the message that the downtown is a vibrant, action-packed and safe space. This perception carries across for both residents and visitors, and consequently it is critical to create a positive experience that reinforces the sense of place that the Town centre is creating.
- » Community Involvement: Bridgewater has an engaged and active community. Keeping this momentum strong while moving forward is critical, as community feedback is essential in not only developing, but also maintaining the success of this plan. Participation in the planning

process, and attendance at community events indicate that people are vested in the future of their Town.

- » Cleanliness: Keeping public areas clear of refuse is important. Clean streets, sidewalks, and walkways are an important indicator of a community's willingness to care, and frequent streetscape recycling facilities support the message that cleanliness matters.

Prior to the implementation of plan projects or components, the Town should collect data for some of these categories (if it does not already exist), to use as a baseline for comparison. At the various implementation milestones, progress can be tracked to ensure that the plan stays focused on the end goals, and any adaptations can be made, as required.



# APPENDICES

## Online Survey Results

**Bridgewater Downtown and Waterfront Master Plan**



1. What is your approximate age?			
		Response Percent	Response Count
under 21		1.8%	4
21-34		20.3%	46
35-49		30.4%	69
50-64		32.2%	73
65+		15.4%	35
answered question			227
skipped question			4

2. Are you			
		Response Percent	Response Count
female		59.8%	134
male		40.2%	90
answered question			224
skipped question			7

3. Do you live in the Downtown area?			
		Response Percent	Response Count
yes		11.2%	25
no		88.8%	199
answered question			224
skipped question			7

4. Do you live within walking distance of Downtown?			
		Response Percent	Response Count
Yes		48.4%	109
No		51.6%	116
answered question			225
skipped question			6

5. Do you drive to Downtown more than you walk to the area?			
		Response Percent	Response Count
yes		90.3%	204
no		9.7%	22
answered question			226
skipped question			5

**6. Do you work in the Downtown area?**

		Response Percent	Response Count
yes		24.1%	54
no		75.9%	170
<b>answered question</b>			<b>224</b>
<b>skipped question</b>			<b>7</b>

**7. How long have you lived or worked in the area?**

		Response Percent	Response Count
less than one year		5.6%	12
one to five years		16.8%	36
more than five years		77.6%	166
<b>answered question</b>			<b>214</b>
<b>skipped question</b>			<b>17</b>

**8. How frequently do you shop in the Downtown?**



		Response Percent	Response Count
daily		15.2%	34
weekly		50.7%	113
monthly		23.3%	52
yearly		7.6%	17
never		3.1%	7
<b>answered question</b>			<b>223</b>
<b>skipped question</b>			<b>8</b>



**9. What day(s) do you generally visit the Downtown? (please select all that apply)**




		Response Percent	Response Count
Monday		48.6%	106
Tuesday		49.5%	108
Wednesday		53.7%	117
Thursday		58.7%	128
Friday		67.4%	147
Saturday		70.6%	154
Sunday		27.5%	60
Never		2.3%	5
<b>answered question</b>			<b>218</b>
<b>skipped question</b>			<b>13</b>




**10. What time of day do you most frequently visit Downtown?**

		Response Percent	Response Count
morning		25.4%	57
afternoon		59.8%	134
evening		14.7%	33
<b>answered question</b>			<b>224</b>
<b>skipped question</b>			<b>7</b>

11. Do you regularly walk in the Downtown?			
		Response Percent	Response Count
yes		48.2%	108
no		51.8%	116
answered question			224
skipped question			7

12. Do you regularly bicycle in the Downtown?			
		Response Percent	Response Count
yes		5.8%	13
no		94.2%	211
answered question			224
skipped question			7

13. How many of the Downtown storefronts, in general, do you consider to be well maintained and appealing?			
		Response Percent	Response Count
all		0.0%	0
most		39.3%	88
very few		56.3%	126
none		4.5%	10
answered question			224
skipped question			7

14. What is your overall opinion of the current appearance of the Downtown area?			
		Response Percent	Response Count
good		5.8%	13
fair		57.6%	129
poor		36.6%	82
answered question			224
skipped question			7

**15. What is your destination(s) when visiting the Downtown?**

		Response Percent	Response Count
bank		70.1%	157
<b>shopping</b>		<b>73.2%</b>	<b>164</b>
restaurant/bar		35.7%	80
friend's home		6.3%	14
professional office (i.e. Lawyer, Financial, Medical)		25.9%	58
parks/open space		28.1%	63
Church		7.1%	16
gym		6.3%	14
theatre		25.9%	58
grocery store		67.9%	152
mall		56.7%	127
I do not visit the downtown		0.9%	2
Other (please specify)		20.5%	46
<b>answered question</b>			<b>224</b>
<b>skipped question</b>			<b>7</b>

**16. How easy is it to find parking in the Downtown?**

		Response Percent	Response Count
easy		34.7%	77
<b>acceptable</b>		<b>58.6%</b>	<b>130</b>
difficult		6.8%	15
<b>answered question</b>			<b>222</b>
<b>skipped question</b>			<b>9</b>

**17. The current directional and shop signage is:**

		Response Percent	Response Count
good		28.8%	64
<b>fair</b>		<b>55.4%</b>	<b>123</b>
poor		15.8%	35
<b>answered question</b>			<b>222</b>
<b>skipped question</b>			<b>9</b>

**18. What other services do you use in the Downtown area? (please select all that apply)**

		Response Percent	Response Count
restaurant/coffee shop		53.7%	115
bar		17.3%	37
library		47.2%	101
automotive		16.4%	35
paint/hardware		49.1%	105
home furnishings		36.4%	78
entertainment		29.0%	62
work		20.6%	44
professional services		31.3%	67
electronics		25.7%	55
<b>grocery store</b>		<b>65.0%</b>	<b>139</b>
clothing		52.8%	113
gifts		54.7%	117
Other (please specify)		8.4%	18
<b>answered question</b>			<b>214</b>
<b>skipped question</b>			<b>17</b>

**19. What other activities, places, or events might bring you to the downtown? (please select all that apply)**

		Response Percent	Response Count
festival		64.1%	134
concert		62.2%	130
playground		18.7%	39
street fair		59.8%	125
splash pad		16.7%	35
trail system		45.9%	96
outdoor plaza		47.8%	100
outdoor dining		67.5%	141
additional shopping		59.8%	125
<b>additional restaurants / bars</b>		<b>72.7%</b>	<b>152</b>
extended operating hours		30.6%	64
Other (please specify)			38
<b>answered question</b>			<b>209</b>
<b>skipped question</b>			<b>22</b>

**20. What amenities would you like to see added to create a better experience for pedestrians? (please select all that apply)**

		Response Percent	Response Count
public benches		52.5%	107
trash receptacles		52.0%	106
bike racks		41.2%	84
seating areas		48.0%	98
public phones		17.2%	35
signage/banners		30.4%	62
better signage		32.8%	67
pedestrian scale lighting		40.7%	83
improved parks		52.5%	107
<b>street trees</b>		<b>57.4%</b>	<b>117</b>
wider sidewalks		28.9%	59
bike lanes		37.7%	77
bus service		50.0%	102
Other (please specify)		24.0%	49
<b>answered question</b>			<b>204</b>
<b>skipped question</b>			<b>27</b>

**21. Which, if any, safety issues do you feel need to be addressed in the Downtown? (please select all that apply)**

		Response Percent	Response Count
hours of commerce		21.4%	45
loitering		25.7%	54
foul language		14.3%	30
perceived lack of care (i.e. litter, pet droppings)		41.0%	86
speed of automobiles		28.6%	60
bicycle parking		30.5%	64
<b>pedestrian crosswalks</b>		<b>42.4%</b>	<b>89</b>
bicycle routes		33.8%	71
accessibility		21.4%	45
lighting and visibility		33.3%	70
sidewalk conditions		38.1%	80
more sidewalks		16.7%	35
Other (please specify)		18.1%	38
<b>answered question</b>			<b>210</b>
<b>skipped question</b>			<b>21</b>

**22. Which, if any, traffic issues do you feel need to be addressed in the Downtown?**

	Response Percent	Response Count
<b>cars driving too fast</b>	50.3%	76
turning lanes	28.5%	43
transport trucks downtown	37.1%	56
access to downtown	26.5%	40
Other (please specify)	28.5%	43
<b>answered question</b>		<b>151</b>
<b>skipped question</b>		<b>80</b>

**23. What facilities or services would you like to see added Downtown?**

	Response Percent	Response Count
<b>first priority</b>	100.0%	147
second priority	84.4%	124
third priority	65.3%	96
fourth priority	44.9%	66
<b>answered question</b>		<b>147</b>
<b>skipped question</b>		<b>84</b>

**24. Do you agree or disagree with the following statements?**

	strongly agree	agree	neutral	disagree	strongly disagree	Rating Count
Revitalizing the Bridgewater Downtown will benefit the community.	73.0% (149)	24.5% (50)	1.5% (3)	0.5% (1)	0.5% (1)	204
Commercial development is a necessary part of this revitalization.	45.5% (92)	38.6% (78)	12.4% (25)	2.5% (5)	1.0% (2)	202
Higher density residential development is a necessary part of this revitalization.	19.5% (39)	24.5% (49)	33.0% (66)	17.0% (34)	6.0% (12)	200
Public access to the riverfront is important.	71.8% (145)	23.3% (47)	3.5% (7)	0.5% (1)	1.0% (2)	202
Signage and wayfinding needs to be improved.	19.6% (38)	39.2% (76)	37.1% (72)	3.6% (7)	0.5% (1)	194
Bridgewater has a strong and clearly defined identity	6.0% (12)	22.5% (45)	28.0% (56)	32.0% (64)	11.5% (23)	200
More riverfront walking trails and bike paths are needed.	54.5% (110)	30.2% (61)	10.4% (21)	3.5% (7)	1.5% (3)	202
Commemorating the Downtown's heritage is important	37.6% (76)	37.6% (76)	23.3% (47)	1.5% (3)	0.0% (0)	202
More parking is necessary close to the waterfront.	16.1% (32)	20.1% (40)	27.6% (55)	23.1% (46)	13.1% (26)	199
The waterfront should be a four-season destination.	57.7% (116)	30.8% (62)	9.0% (18)	2.5% (5)	0.0% (0)	201
"Green" design principles are important in revitalizing the Downtown.	50.5% (102)	30.2% (61)	15.8% (32)	3.0% (6)	0.5% (1)	202
Addressing the impacts of flooding and climate change is an important consideration.	33.0% (67)	38.9% (79)	23.6% (48)	3.4% (7)	1.0% (2)	203
<b>answered question</b>						<b>205</b>
<b>skipped question</b>						<b>26</b>

**25. How important are the following in regards to this overall study?**

	Very Important	Important	Relevant	Not Relevant	Rating Count
Integration of active transportation (i.e. bikes and trails)	<b>51.2% (103)</b>	31.3% (63)	13.9% (28)	3.5% (7)	201
Retail development	<b>56.9% (116)</b>	32.8% (67)	9.3% (19)	1.0% (2)	204
Office development	27.8% (55)	<b>35.4% (70)</b>	32.8% (65)	4.0% (8)	198
Residential development	23.5% (47)	<b>37.0% (74)</b>	29.0% (58)	10.5% (21)	200
Signage and wayfinding	19.4% (38)	<b>40.8% (80)</b>	33.7% (66)	6.1% (12)	196
Attraction and retention of tourists	<b>51.2% (104)</b>	36.5% (74)	9.9% (20)	2.5% (5)	203
Protection of natural resources and habitat	<b>61.4% (124)</b>	30.2% (61)	7.9% (16)	0.5% (1)	202
New or expanded open space	<b>45.3% (91)</b>	34.8% (70)	14.9% (30)	5.0% (10)	201
Parking	20.5% (41)	35.0% (70)	<b>35.5% (71)</b>	9.0% (18)	200
			<b>answered question</b>		<b>204</b>
			<b>skipped question</b>		<b>27</b>

**26. What improvements/design elements would you like to see included in a waterfront design**

	High Priority	Medium Priority	Low Priority	Rating Count
Public Boardwalk	<b>62.7% (126)</b>	31.8% (64)	5.5% (11)	201
Amphitheater/performance venue	<b>41.2% (82)</b>	40.7% (81)	18.1% (36)	199
Marina	<b>46.7% (93)</b>	36.7% (73)	16.6% (33)	199
Public washrooms	<b>64.7% (130)</b>	28.9% (58)	6.5% (13)	201
Open Park Space	<b>55.5% (111)</b>	33.5% (67)	11.0% (22)	200
Play areas	<b>48.3% (97)</b>	38.3% (77)	13.4% (27)	201
Canteen/Cafe	<b>57.1% (113)</b>	30.3% (60)	12.6% (25)	198
		Other (please specify)		29
		<b>answered question</b>		<b>204</b>
		<b>skipped question</b>		<b>27</b>

**27. Please add any additional comments you may have.**

	Response Count
	55
<b>answered question</b>	<b>55</b>
<b>skipped question</b>	<b>176</b>

Page 3, Q15. What is your destination(s) when visiting the Downtown?		
1	Port	Nov 28, 2012 7:55 AM
2	My business is downtown	Nov 19, 2012 7:56 PM
3	Library!	Nov 19, 2012 6:49 PM
4	just walking around	Nov 19, 2012 10:46 AM
5	KD Micro, dressmaker	Nov 19, 2012 9:02 AM
6	driving kids to school	Nov 16, 2012 1:33 PM
7	library	Nov 16, 2012 1:05 PM
8	Library	Nov 16, 2012 12:23 PM
9	library	Nov 16, 2012 12:13 PM
10	work	Nov 16, 2012 11:28 AM
11	library	Nov 13, 2012 11:23 AM
12	Library	Nov 13, 2012 7:54 AM
13	Work	Nov 12, 2012 12:45 PM
14	work	Nov 10, 2012 6:29 AM
15	library	Nov 8, 2012 2:52 PM
16	Library	Nov 7, 2012 1:13 PM
17	walk the dog, view the river.	Nov 7, 2012 11:54 AM
18	I volunteer at various agencies	Nov 5, 2012 2:14 PM
19	place of employment	Nov 5, 2012 11:04 AM
20	library and farmers market every saturday	Nov 2, 2012 7:39 PM
21	Farmer's Market	Nov 2, 2012 6:11 PM
22	beautiful waterfront WASTED!!!	Nov 1, 2012 1:10 PM
23	Library	Nov 1, 2012 10:43 AM
24	Farmer's market	Nov 1, 2012 9:34 AM
25	library	Oct 31, 2012 3:30 PM
26	library	Oct 31, 2012 3:07 PM
27	Library	Oct 31, 2012 2:28 PM

Page 3, Q15. What is your destination(s) when visiting the Downtown?		
28	Market	Oct 31, 2012 1:49 PM
29	The boat dock	Oct 31, 2012 1:15 PM
30	Library! The Ark!	Oct 31, 2012 12:14 PM
31	I would visit others if they were more appealing and if the open spaces were maintained for proper watershed management and restoration of our beautiful LaHave River!	Oct 31, 2012 11:49 AM
32	library	Oct 30, 2012 4:07 AM
33	Library	Oct 29, 2012 12:40 PM
34	Farmers Market	Oct 29, 2012 11:01 AM
35	work - if that counts (on previous page, work seemed to qualify)	Oct 28, 2012 6:50 AM
36	Exercise classes throughout town	Oct 27, 2012 11:46 AM
37	work	Oct 27, 2012 6:56 AM
38	property maintenance as landlord	Oct 26, 2012 2:19 PM
39	Gift store	Oct 26, 2012 1:48 PM
40	Farmers market during the summer	Oct 26, 2012 1:26 PM
41	work	Oct 26, 2012 10:57 AM
42	Depends on whether you consider the mall "downtown". I do not.	Oct 26, 2012 9:53 AM
43	work (office)	Oct 26, 2012 9:26 AM
44	Town Hall	Oct 24, 2012 7:08 AM
45	Work	Oct 24, 2012 3:39 AM
46	To meet with Contractors/Builders	Oct 23, 2012 1:14 PM

Page 4, Q18. What other services do you use in the Downtown area? (please select all that apply)

1	BANKING	Dec 6, 2012 6:41 PM
2	footwear	Nov 28, 2012 7:52 PM
3	farmers market	Nov 22, 2012 11:49 AM
4	books	Nov 10, 2012 6:30 AM
5	tourist attractions	Nov 7, 2012 11:58 AM
6	farmers market	Nov 2, 2012 7:40 PM
7	area not conducive to recreation...more parks, restaraunts etc. PLEASE	Nov 1, 2012 1:13 PM
8	MLA office	Nov 1, 2012 9:36 AM
9	There are no grocery stores why would you have this on the list?	Nov 1, 2012 4:32 AM
10	fitness	Oct 31, 2012 1:53 PM
11	Boat dock	Oct 31, 2012 1:16 PM
12	Volunteer work. Community Shared Agriculture pick-up at The Ark.	Oct 31, 2012 12:16 PM
13	trail system	Oct 29, 2012 3:59 AM
14	meetings	Oct 27, 2012 4:35 PM
15	beauty	Oct 27, 2012 7:38 AM
16	Inspection and maintenance	Oct 26, 2012 2:21 PM
17	bakery, bank	Oct 26, 2012 9:29 AM
18	Town Planning Department	Oct 23, 2012 1:19 PM

Page 4, Q19. What other activities, places, or events might bring you to the downtown? (please select all that apply)

1	More KID friendly activities....splash pad is wonderful!	Dec 9, 2012 10:15 AM
2	Nothing else brings me downtown.	Nov 30, 2012 7:59 AM
3	actual good restaurants, which currently do not exist in Bridgewater, concerts which are not country music	Nov 22, 2012 11:49 AM
4	farmers market	Nov 19, 2012 3:54 PM
5	art/artisan galleries	Nov 19, 2012 9:53 AM
6	Don't know	Nov 19, 2012 5:06 AM
7	outdoor market	Nov 18, 2012 2:33 AM
8	Boardwalk along river	Nov 16, 2012 12:24 PM
9	walking and bike trails	Nov 7, 2012 1:59 PM
10	Comfortable and creative gathering places (placemaking)	Nov 7, 2012 1:15 PM
11	museums, boat tours, boat regalia, garden tours, a park on the waterfront	Nov 7, 2012 11:58 AM
12	Skatepark	Nov 5, 2012 7:54 AM
13	canoeing or kayaking events	Nov 3, 2012 5:40 AM
14	get rid of parkade and develop waterfront	Nov 3, 2012 3:24 AM
15	good coffee shop, nice food, pleasant environment.	Nov 2, 2012 8:26 PM
16	Need to sign the trail system, both informational and directional!	Nov 2, 2012 4:01 PM
17	concrete skateboard park	Nov 2, 2012 3:03 PM
18	Mall complex faces AWAY from riverfront.... how ridiculous...potentially beautiful space completely wasted..what a lack of foresight.	Nov 1, 2012 1:13 PM
19	marina, permanent farmer's market location, footbridges	Nov 1, 2012 9:36 AM
20	Stores are not opened in the evening to all low for shopping	Nov 1, 2012 4:32 AM
21	farmers market	Oct 31, 2012 3:31 PM
22	good coffee shop to replace Wildwood Cafe	Oct 31, 2012 3:09 PM
23	Did I say extended hours? Merchants must realize some people with money to spend might actually WORK between 8:00 am - 5:00 or 6:00 pm!!!!	Oct 31, 2012 1:53 PM
24	Boat Services	Oct 31, 2012 1:16 PM
25	we need another bar in this town	Oct 31, 2012 12:26 PM

**Page 4, Q19. What other activities, places, or events might bring you to the downtown? (please select all that apply)**

26	A decent restaurant would be nice. Right now, I consider only the River Pub and Friends Cafe to be worth visiting for a meal. (Must try Food Revolution...)	Oct 31, 2012 12:16 PM
27	Safe Cycling/pedestrian access and improved stores/frontage	Oct 31, 2012 11:52 AM
28	Keep out big box stores and making edible gardens, reforest much of the free spaces, and get to work on real riparian buffer zones on both sides of the LaHave River!	Oct 31, 2012 11:51 AM
29	Paddle boats, canoes, kayaks	Oct 30, 2012 12:57 PM
30	desperately need a good restaurant on King St.	Oct 29, 2012 3:42 PM
31	A good restaurant and some descent shops. Not a bargain centre or pizza place.	Oct 29, 2012 11:47 AM
32	The shopping in Bridgewater is pathetic!	Oct 29, 2012 5:04 AM
33	Get rid of the things blocking the river usage/ alter them to make them more appealing. Fire Jerome Richard. :)	Oct 28, 2012 6:42 AM
34	meetings	Oct 27, 2012 4:35 PM
35	more access to waterfront	Oct 26, 2012 2:07 PM
36	More open space along the water	Oct 24, 2012 7:10 AM
37	More parks and ublic spaces	Oct 24, 2012 3:40 AM
38	Meeting Contractors at my property on Lahave Street	Oct 23, 2012 1:19 PM

**Page 5, Q20. What amenities would you like to see added to create a better experience for pedestrians? (please select all that apply)**

1	low impact lighting everywere	Dec 27, 2012 6:03 AM
2	Connectivity to parland and improvement or removal of south parkade	Nov 23, 2012 7:08 AM
3	An outdoor "green" gym.	Nov 22, 2012 7:13 AM
4	And bus route to rissers beach daily in am and pm return during summer months; also a bus route to the lifestyle Centre quarterly and daily.	Nov 19, 2012 4:28 PM
5	River trail to shipyard landing	Nov 19, 2012 3:57 PM
6	Treed boulevards to separate the pedestrian from traffic and parking lots	Nov 19, 2012 10:01 AM
7	Not sure	Nov 19, 2012 5:14 AM
8	Try to find Main Street funding and at least paint the storefronts wonderful colours and get new signage for every building - it is depressing to go to the main street in B'Water - totally uninspiring - even a long-standing store like Cleve's has old fading signage - who wants to shop in a place where the stores are so badly taken care of....no pride of property is the number one impression a shopper or would-be shopper gets. There used to be a Main Street Program delivered by Heritage Canada - wonder if it is still around? B'Water is all malls, cars, parking lots on one side of the river and dilapidated looking buildings on the other - very uninspiring - you have a beautiful river to work with - there should life and joy on the main street - at least PAINT the store fronts and up date stores SIGNS so they are appealing to the shopper. Further when you mention restaurants - what are you talking about - there is no place to eat except fast food joints in B'Water. Where is the culture? Try to find the DesBrisay if you don't have a vague idea of where it is - try to find an art gallery - there isn't one that I can figure out. What "theatre" would that be you mention - are you talking about cinema? There can't be any live theatre because I have never seen or heard of a theatre there. It's a shame the town is such a wreck - you even have 'a biker gang' on the main street - good grief. Can't the police move them out - it's frightening. It is probably one of the unfriendliest looking, dilapidated "shopping" areas I have ever seen. B'Water sits on a gem of a river. Lahave - gorgeous river with so much to offer in views and enjoyment - all the buildings have their backs to the river - why? I don't know how the businesses that are there survive. People from all around the area outside B'Water would come there to shop if it looked like there was any downtown shopping to be had. Yes, please do something for poor old B'Water - it's begging for a substantial update.	Nov 18, 2012 2:48 AM
9	Outdoor cafe area/a restaurant district/ and a fashion district perhaps	Nov 17, 2012 6:48 PM
10	Remove parkades and develop river front with boardwalk/trails. Reconfigure parking to angle parking on King St	Nov 16, 2012 12:26 PM
11	More sidewalk/crosswalk markings when there is a division/break in the road intersecting another road.	Nov 16, 2012 11:32 AM
12	A barrier between pedestrians and the vehicles on the 'old' bridge.	Nov 13, 2012 8:30 AM
13	Create a waterfront area where shops and walking trails are located instead of	Nov 7, 2012 2:04 PM

**Page 5, Q20. What amenities would you like to see added to create a better experience for pedestrians? (please select all that apply)**

	parking garages.	
14	Gathering places, places that draw people in/out, creative spaces. More access or views of the river, green space instead of parking lots that extend down to the river where people can put in canoes/kayaks. Clean the river so we can swim too	Nov 7, 2012 1:22 PM
15	a public garden overlooking the water with sitting areas, boat tours	Nov 7, 2012 12:06 PM
16	marina that would enhance ship yard landing.	Nov 5, 2012 2:20 PM
17	safer pedestrian ccross walks cars continue to drive through after the light as been pressed they dont see what is takng place	Nov 5, 2012 11:14 AM
18	The town could be more attractive in the summer with sidewalk flower pots and greenery	Nov 4, 2012 7:18 AM
19	display windows on the back of mall for window-shopping while strolling along river	Nov 2, 2012 6:17 PM
20	coloured crosswalks, walking routes marked/signed Tear down a parkade and add some pedestrian throughways	Nov 2, 2012 4:10 PM
21	the view from one side of the river consists of: the back of the mall, a wharf with rusty boats, wendy's, a shell station, the back of TD bank. this is really ugly and should be remedied.	Nov 2, 2012 3:28 PM
22	concrete skateboard park	Nov 2, 2012 3:05 PM
23	dog friendly areas	Nov 1, 2012 3:37 PM
24	Bike- and pedestrian-oriented lanes with very small storefronts, not necessarily always open, for artisan or specialty shops that cannot afford full storefronts and do not need regular drive-by traffic to be viable.	Nov 1, 2012 9:42 AM
25	Bus Stops (pull in area)	Nov 1, 2012 3:00 AM
26	public transit	Nov 1, 2012 2:34 AM
27	No body rides bikes that has money	Oct 31, 2012 1:18 PM
28	We totally need bus service on the south shore. I would love to be able to take a bus from my home near LaHave to and around B-water!	Oct 31, 2012 12:21 PM
29	Lots more vegetation! Edible gardens, reforested areas, preserved marshlands, and serious riparian buffer zones on both sides of the LaHave River to keep our river healthy and beautiful. The lack of this proper management of our natural resources makes Bridgewater ugly and embarrassing to bring people to and spend time in! Time to get into the 21st century, face climate change, reverse environmental degradation, and represent the needs and desires of youth for our future.	Oct 31, 2012 12:00 PM
30	People mover shuttle service, set route, quick on and off, donation based. Wifi	Oct 30, 2012 1:30 PM

**Page 5, Q20. What amenities would you like to see added to create a better experience for pedestrians? (please select all that apply)**

31	Concrete sidewalks to make for better wintertime snow/ice removal and easier passage for pedestrians.	Oct 30, 2012 6:56 AM
32	Purchase & demolish some waterfront buildings & open up the river, perhaps add a boardwalk. Try to get stores to improve their appearance. Try to get businesses into the empty stores.	Oct 30, 2012 4:14 AM
33	outdoor water taps	Oct 29, 2012 4:06 PM
34	Merchants maintaining existing business. Maybe some new business.	Oct 29, 2012 11:50 AM
35	My daughter complains that cars dont stop for her when she tries to cross at the Dairy Queen/automotive corner both times of day when she walks to/from school!	Oct 29, 2012 11:24 AM
36	outdoor cafes pedestrian only area	Oct 29, 2012 11:08 AM
37	Replace King street river parking lots with two tier pedestrian friendly areas	Oct 29, 2012 11:02 AM
38	boardwalks on the water with shops, etc	Oct 29, 2012 6:30 AM
39	public washroom	Oct 27, 2012 4:38 PM
40	Walking bridge across the river to complete a loop up one side of the river and down the other.	Oct 27, 2012 2:46 AM
41	More appealing store fronts, lower assessments for business which would make it more affordable to fill vacant spaces, and attract appealing businesses	Oct 26, 2012 2:33 PM
42	get rid of the south parkade and put in diagnol parking and a bike lane close to the water(inside the parking areas.	Oct 26, 2012 2:13 PM
43	Keep the parkades. There won't be enough parking without them. Just clean them up. In summer add some plants, and maybe Christmas trees for the winter. They could be made attractive with a little imagination.	Oct 26, 2012 1:53 PM
44	traffic calming measures days that the street is closed to vehicle traffic street food	Oct 26, 2012 11:03 AM
45	Buildings well maintained, painted, weed wacked, garbage removed, better lighting, heritage look maintained.	Oct 26, 2012 9:59 AM
46	more crosswalks	Oct 26, 2012 9:21 AM
47	area where we can get a snack & beverage and just sit outside and enjoy the river vs bringing our own beverage & sitting in the park by the river. Grandson loves the fountain...a downtown land-fountain/splash area would be awesome!	Oct 26, 2012 4:35 AM
48	Perhaps an small outdoor amphitheatre or performance space where the southern parkade is located; More floating wharfs for small boats	Oct 24, 2012 9:17 AM
49	To consider installing a lighted cross walk about in the middle of our property facing on Lahave Street. It would allow safe access to the side walk and future	Oct 23, 2012 1:47 PM

Page 5, Q20. What amenities would you like to see added to create a better experience for pedestrians? (please select all that apply)

common-use floating dock.

Page 5, Q21. Which, if any, safety issues do you feel need to be addressed in the Downtown? (please select all that apply)

1	low impact lighting - you can see the skylight for miles; this is unacceptable	Dec 27, 2012 6:03 AM
2	I do not see any safety issues.	Nov 30, 2012 9:03 AM
3	work on getting appealing tenants into empty commercial space and improve appearance of some pretty unattractive facades.	Nov 26, 2012 1:01 AM
4	People from neighbouring business (owners and their customers) standing beside my shop smoking 1/2 the day. A real safety issue is the crosswalk at King and Phoenix. Many pedestrians have been hit when crossing there even though the crosswalk lights have been added. It would help if there was no longer 2 lanes of traffic heading North (sometimes a car doesn't realize that the person in the next lane has stopped for a pedestrian and so they try to proceed in the other lane. That isn't always the issue though because the accident can come from South bound traffic as well.	Nov 19, 2012 8:28 PM
5	Improved video surveillance to curb vandalism	Nov 19, 2012 5:14 AM
6	Motorcycle club and tattoo parlours -- bad company hanging out	Nov 16, 2012 12:26 PM
7	A barrier between pedestrians and the vehicles on the 'old' bridge. If someone stumbles or trips they could fall directly into traffic. This is a big concern for those of us with children using the bridge to walk to school.	Nov 13, 2012 8:30 AM
8	Garbage, compost, cardboard, etc from businesses	Nov 13, 2012 8:08 AM
9	Lighted crosswalks. Crosswalk closer to the Farmer's Market.	Nov 8, 2012 3:10 PM
10	Perhaps make one way to lessen traffic and add cycling path	Nov 8, 2012 2:55 PM
11	The parking lots are unsightly and prevent views of the river. People drive very fast on King street. More views of the river.	Nov 7, 2012 1:22 PM
12	road condition	Nov 5, 2012 7:56 AM
13	None	Nov 4, 2012 7:18 AM
14	police foot patrols	Nov 3, 2012 3:25 AM
15	tattoo parlours in the downtown areas, and Motor cycle group do not promote positive attitude in downtown area. These make the downtown area look run down.	Nov 3, 2012 3:18 AM
16	Speed of vehicles	Nov 2, 2012 2:15 PM
17	Aggressive presence of police force... it isn't that I don't want to meet a police officer, it's that they just drive around town in giant dodge chargers looking like something out of a corny CSI show. Are those cars necessary? Must they add to the traffic?	Nov 2, 2012 1:56 PM
18	The lighting for crosswalks is horrendous! There needs to be more light on the ends of the crosswalk to see if someone is entering	Nov 2, 2012 7:52 AM

**Page 5, Q21. Which, if any, safety issues do you feel need to be addressed in the Downtown? (please select all that apply)**

19	none	Nov 1, 2012 3:37 PM
20	Bus service - a way to get in and out other than by car. Taxi service also needs to be more available and accessible without a phone at guaranteed rates to specific destinations.	Nov 1, 2012 9:42 AM
21	Engage youth, rather than alienate	Oct 31, 2012 2:00 PM
22	Get rid of bikers, they are a hazard to themselves	Oct 31, 2012 1:18 PM
23	I feel pretty safe in downtown Bridgewater.	Oct 31, 2012 12:21 PM
24	More green spaces filled with native plants; more spaces that preserve environment - and ecosystem habitats, such as marshlands and the riparian zone of the LaHave River!	Oct 31, 2012 12:00 PM
25	The parkades are a disgrace, the bridge is a disgrace	Oct 31, 2012 4:02 AM
26	Very specifically, the walk ways on the Old Bridge need to be dealt with in some fashion - most hazardous part of the downtown in my opinion.	Oct 30, 2012 6:56 AM
27	The buildings and street in general on south King St. just look shoddy.	Oct 29, 2012 3:51 PM
28	Buildings in poor repair.	Oct 29, 2012 11:50 AM
29	Its safer to walk than bike from one side of the downtown to the other. Even with the comments i made previously. I live on stretch drive so its not far to walk.	Oct 29, 2012 11:24 AM
30	ugly parkades	Oct 29, 2012 10:36 AM
31	N/A	Oct 29, 2012 10:11 AM
32	lack of public transportation	Oct 29, 2012 7:35 AM
33	There should be more cross walks if possible. A lot of Jay walking.	Oct 28, 2012 6:46 AM
34	designated crosswalk at north parkade - not necessary to have it lighted but designated.	Oct 27, 2012 7:43 AM
35	Please paint the bandstand at Shipyards landing. Make it look as pretty as the small one over the bank that one rarely can see, since it is hidden from the street. Make King street one way with angled parking and get take out the parkade since it is totally an eyesore. Let's make use of the river bank as it once was years ago. Make Bridgewater pretty again.	Oct 27, 2012 4:43 AM
36	smoking continues to be a problem in the downtown area despite the by-law which does not appear to be enforced.	Oct 26, 2012 11:15 AM
37	Parkade over the river is wonderful for easy access parking but a poor use of such a beautiful area which would be beautiful as a boardwalk/splashpark for kiddies ( with proper fencing of waterfront of course) This then begs the question...where then would I park?	Oct 26, 2012 4:35 AM

**Page 5, Q21. Which, if any, safety issues do you feel need to be addressed in the Downtown? (please select all that apply)**

38	Driver awareness on the rights of pedestrians and cyclists is a major concern in Bridgewater. Drivers often seem clueless, or at the very least in a rush to pass through intersections even though traffic isn't too heavy; I know I've almost been a victim of inattentive drivers while crossing the street in the downtown many, many times.	Oct 24, 2012 3:50 AM
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Page 5, Q22. Which, if any, traffic issues do you feel need to be addressed in the Downtown?

1	Bridgewater NEEDS to be double laned!!!! We have city traffic with town roads!	Nov 28, 2012 4:01 PM
2	I need transport trucks to be able to deliver supplies to my business. Another issue is that I can not turn left on King or go straight into the parkade off Phoenix. So several times a day i am forced to turn right and go into the little parking space beside Kos restaurant in order to zig zag back into the parkades. My loading door is on Phoenix so the option of traveling a different route isn't there. I heard that this right turn only was put in place for the sake of the police station which has been relocated a number of years ago.	Nov 19, 2012 8:28 PM
3	Some crosswalks do not seem to be visible enough to drivers as they often do not stop for pedestrians	Nov 19, 2012 5:14 AM
4	bike access needs to be safer to cross the old bridge for kids going to school. Two options are on the elevated sidewalk where pedestrians are or holding up traffic on bridge	Nov 16, 2012 1:44 PM
5	Need to be make pedestrian-friendly	Nov 16, 2012 12:26 PM
6	pedestrians j-walking	Nov 14, 2012 8:22 AM
7	None	Nov 13, 2012 11:31 AM
8	The intersection by the new bridge needs pedestrian walk/don't walk lights to assist pedestrians (especially children) to cross the street. When coming off the bridge and trying to cross the street to go up the hill from Superstore it is very dangerous with the cars from the bridge turning right.	Nov 13, 2012 8:30 AM
9	parking	Nov 8, 2012 9:41 AM
10	When the road is closed for the Growing Green festival its such a beautiful space. SO regular street closures in the summer	Nov 7, 2012 1:22 PM
11	pedestrians crossing the street can hold and back up traffic at times, green lights can be missed. transport trucks parking on the street are a nuisance as well.	Nov 4, 2012 3:37 PM
12	Add another lane to the old bridge	Nov 4, 2012 7:18 AM
13	public transportation	Nov 2, 2012 6:17 PM
14	Autos sharing the the space with other users (pedestrains, cyclists)	Nov 2, 2012 4:10 PM
15	none	Nov 1, 2012 3:37 PM
16	Downtown is not for through traffic, period. A higher bridge capable of accomodating sailboats underneath further down river is probably the only long term solution.	Nov 1, 2012 9:42 AM
17	Public Transit to cut down on the insane amount of traffic this area has. We are in the dark ages.	Oct 31, 2012 3:39 PM
18	transit system	Oct 31, 2012 3:10 PM
19	Better boating, move underwater rocks	Oct 31, 2012 1:18 PM

Page 5, Q22. Which, if any, traffic issues do you feel need to be addressed in the Downtown?

20	Excessive traffic lights where not needed or necessary!! Backs up traffic a great deal and is HIGHLY inconvenient!	Oct 31, 2012 12:39 PM
21	I think traffic flows pretty well, except maybe near the mall by the old bridge. But, it's livable.	Oct 31, 2012 12:21 PM
22	Bicycle lanes and skateboard and longboard accessibility that is SAFE from vehicular traffic! Also, we NEED to have public bus infrastructure that connects to all parts of the town, the schools and college, and those who need to come to Bridgewater on a regular basis from all other parts of the municipality!	Oct 31, 2012 12:00 PM
23	Cycle/pedestrian Friendly and Stores, that Market Local products and art. A cafe OTHER THAN Timhortons	Oct 31, 2012 11:55 AM
24	A traffic light at the top of Silvers Hill and North Street	Oct 30, 2012 1:30 PM
25	Flow of traffic - one-way streets, when done right and supported by studies, make a safer environment for pedestrians and drivers.	Oct 30, 2012 6:56 AM
26	Traffic is not an issue except for the hideous parking lots.	Oct 30, 2012 4:14 AM
27	Parking.	Oct 29, 2012 11:50 AM
28	Traffic circles	Oct 29, 2012 11:02 AM
29	parkades	Oct 29, 2012 10:36 AM
30	N/A	Oct 29, 2012 10:11 AM
31	streets are very congested-takes too long to get around. need wider roads and bridges	Oct 29, 2012 6:30 AM
32	Overall, the town is doing a great job. My suggestions are though, there is too much trafficking on LaHave St and this could be addressed with a round-about at Aberdeen and Glen Allan. Also, I see massive cutting of trees to put in subdivisions. I have lived in many places, and the more progressive leave their forested areas intact. I have had my business in the downtown area for about 5 years, and see the town as a great alternative to locating in Halifax. More advertising of Bridgewater as a great place to work and live would be welcome.	Oct 29, 2012 3:35 AM
33	Tooooooo much construction blocking SEVERAL major roads at one time. One at a time please.	Oct 28, 2012 6:46 AM
34	bus transit	Oct 27, 2012 4:38 PM
35	Additional lights at the Tim Hortons near the NSCC and up near exit #13 where the Cross Road and Victoria Rd. Both areas are often difficult to exit.	Oct 27, 2012 11:53 AM
36	Crosswalk at BMO is totally dangerous because people press the button and go without looking. If the town were to monitor the other crosswalk at the old bridge, they would make a fortune in fines. People drive through there when the crosswalk is being used; they do not stop before turning right on the red light. It is the most dangerous place in town.	Oct 27, 2012 4:43 AM

**Page 5, Q22. Which, if any, traffic issues do you feel need to be addressed in the Downtown?**

37	Lack of driver knowledge of road rules ie. when you are no longer facing a flashing green light you no longer have the right-of-way. This problem is compounded by lack of enforcement by police. More effort is put into issuing parking tickets to people who are patronizing downtown businesses than the safety of drivers in the downtown.	Oct 26, 2012 5:01 PM
38	More visible crosswalks	Oct 26, 2012 4:28 PM
39	it is hard to change the other three above.	Oct 26, 2012 2:13 PM
40	The traffic lights are programmed badly.	Oct 26, 2012 11:13 AM
41	please don't add a turning lane. Slow it all down!	Oct 26, 2012 11:03 AM
42	Divert regional traffic away from places where we want to create pedestrian environments; Rethink parking - maybe angled parking with force drivers to slow down?	Oct 24, 2012 9:17 AM
43	Need to find ways to slow traffic down and raise awareness on pedestrian and cyclist safety. Reducing the amount of through traffic in general would be great.	Oct 24, 2012 3:50 AM

**Page 5, Q23. What facilities or services would you like to see added Downtown?**

first priority		
1	playground	Jan 17, 2013 4:52 AM
2	low impact lighting	Dec 27, 2012 6:03 AM
3	entertainment	Dec 6, 2012 6:44 PM
4	bike lanes	Dec 2, 2012 6:04 PM
5	There are no services of facilities	Nov 30, 2012 9:03 AM
6	eateries of good quality,	Nov 28, 2012 7:54 PM
7	Double Lane Roads	Nov 28, 2012 4:01 PM
8	Marina	Nov 28, 2012 7:59 AM
9	enjoy the accessibility of the library and would like a CAP internet point maintained if nothing else when the library moves	Nov 26, 2012 1:01 AM
10	non-fast food restaurant	Nov 25, 2012 6:06 PM
11	good eating places	Nov 25, 2012 10:25 AM
12	Restaurant aside from pizza/burgers	Nov 23, 2012 7:08 AM
13	Cultural	Nov 23, 2012 6:00 AM
14	Bars	Nov 23, 2012 4:39 AM
15	skate park for the kids	Nov 23, 2012 3:14 AM
16	Good affordable ethnic restaurants	Nov 22, 2012 11:52 AM
17	More flexible housing options	Nov 22, 2012 9:44 AM
18	Outdoor Gym	Nov 22, 2012 7:13 AM
19	Cafes!!!!	Nov 19, 2012 8:28 PM
20	Cafe/meeting area	Nov 19, 2012 6:55 PM
21	Library to remain downtown	Nov 19, 2012 4:28 PM
22	restaurants	Nov 19, 2012 3:57 PM
23	affordable, accessible places for people to meet, informally (friends) and formally (meetings)	Nov 19, 2012 10:01 AM
24	More access to the waterfront from the mall (like a restaurant)	Nov 19, 2012 9:05 AM
25	Improved video surveillance at night	Nov 19, 2012 5:14 AM

Page 5, Q23. What facilities or services would you like to see added Downtown?		
26	river front activities	Nov 18, 2012 4:22 PM
27	improved parking arcade	Nov 18, 2012 12:53 PM
28	Fresh paint on all buildings	Nov 18, 2012 2:48 AM
29	Restaurant district outdoor cafe seating on water front	Nov 17, 2012 6:48 PM
30	Better public transport! Buses.	Nov 16, 2012 2:09 PM
31	Much more bike friendly for the teens to commute to school safely	Nov 16, 2012 1:44 PM
32	restaurant/café with patio to sit close to the water	Nov 16, 2012 1:11 PM
33	boardwalk on river side	Nov 16, 2012 12:26 PM
34	Better restaurants for fine dining	Nov 16, 2012 12:17 PM
35	the unsightly look of the buildings	Nov 16, 2012 11:54 AM
36	sidewalk conditions	Nov 16, 2012 11:32 AM
37	restaurant variety	Nov 16, 2012 7:58 AM
38	quality restaurant	Nov 14, 2012 8:22 AM
39	Bars and restaurants	Nov 13, 2012 11:31 AM
40	more stores, cafes, and boutiques	Nov 13, 2012 8:30 AM
41	Higher end resturant	Nov 13, 2012 8:08 AM
42	public transit	Nov 10, 2012 6:33 AM
43	Restaurants	Nov 8, 2012 3:29 PM
44	healthy restaurant	Nov 8, 2012 3:10 PM
45	cafe	Nov 8, 2012 2:55 PM
46	restaurants	Nov 8, 2012 9:41 AM
47	more empty spaces filled by merchants	Nov 8, 2012 6:45 AM
48	Coffee shop other than Tim Hortons	Nov 7, 2012 2:04 PM
49	A welcoming creative cafe space	Nov 7, 2012 1:22 PM
50	the buildings cleaned and painted	Nov 7, 2012 12:06 PM
51	more parking	Nov 7, 2012 11:18 AM
52	acces to the rivver	Nov 7, 2012 7:22 AM

Page 5, Q23. What facilities or services would you like to see added Downtown?		
53	Good restaurant	Nov 6, 2012 12:28 PM
54	More shops utilizing empty building space	Nov 6, 2012 9:56 AM
55	lots of parking with wider spaces	Nov 6, 2012 7:00 AM
56	good cafe	Nov 5, 2012 12:58 PM
57	more restaurants	Nov 5, 2012 11:14 AM
58	a nice restaurant	Nov 3, 2012 1:04 PM
59	cafe	Nov 3, 2012 12:53 PM
60	events	Nov 3, 2012 5:44 AM
61	public transportation	Nov 2, 2012 6:17 PM
62	Public greenspace behind mall/along river	Nov 2, 2012 6:11 PM
63	grass weeds around buildings and sidewalks	Nov 2, 2012 5:37 PM
64	pedestrian and cycling related facilities and amenities	Nov 2, 2012 4:10 PM
65	patio dining (licenced)	Nov 2, 2012 3:28 PM
66	concrete skateboard park	Nov 2, 2012 3:05 PM
67	Swimming Pool	Nov 2, 2012 2:15 PM
68	Green space	Nov 2, 2012 2:15 PM
69	Family Resource Centre, including a drop in centre for youth.	Nov 2, 2012 1:56 PM
70	increase shops on King street(make opportunities for businesses to set up)	Nov 2, 2012 7:52 AM
71	restaurants	Nov 1, 2012 3:37 PM
72	bus services	Nov 1, 2012 2:56 PM
73	Cafe	Nov 1, 2012 10:46 AM
74	bus service and stops	Nov 1, 2012 9:42 AM
75	Up to Date downtown. Looks like it is from the 60's	Nov 1, 2012 4:46 AM
76	Shops with standard hours to encourage shopping after work.	Nov 1, 2012 4:34 AM
77	Remove old unusable buildings	Nov 1, 2012 4:07 AM
78	Public Transit	Nov 1, 2012 3:00 AM
79	public transit	Nov 1, 2012 2:34 AM

Page 5, Q23. What facilities or services would you like to see added Downtown?

80	More access to the river for pedestrians	Oct 31, 2012 5:02 PM
81	Get rid of that ugly as sin mall and put in a park so the public can actually use the riverfront in the downtown core.	Oct 31, 2012 3:39 PM
82	access to the river with a nice walkway	Oct 31, 2012 3:34 PM
83	transit	Oct 31, 2012 3:10 PM
84	Public transit	Oct 31, 2012 2:30 PM
85	Concert, entertainment venue	Oct 31, 2012 2:00 PM
86	Better docks within the town, not down the river.	Oct 31, 2012 1:18 PM
87	More attractive, safe, trail behind the mall along the river. Somehow mitigate the mistake that the mall was.	Oct 31, 2012 1:08 PM
88	More shopping variety	Oct 31, 2012 12:39 PM
89	another bar is needed for young adults	Oct 31, 2012 12:28 PM
90	I am really sad that the library is moving out of the downtown.	Oct 31, 2012 12:21 PM
91	Restore banks of the LaHave River with native plant species and culverts for ducks and other wild species to prevent roadkill	Oct 31, 2012 12:00 PM
92	Bicycle access and racks,	Oct 31, 2012 11:55 AM
93	continue to promote development of mixed commercial/residential buildings. Prime example being former LaHave Furnishings structure, commercial on lower king street level, residential upper 3 floors.	Oct 31, 2012 6:23 AM
94	Canoe, kayak, paddle boat rentals	Oct 30, 2012 1:30 PM
95	Bus service	Oct 30, 2012 6:56 AM
96	The town should proactively try to maintain "anchor" facilities -- as it failed to do with the post office and library	Oct 30, 2012 4:14 AM
97	a cafe would be nice	Oct 29, 2012 7:06 PM
98	child park or playground	Oct 29, 2012 4:06 PM
99	a good restaurant/cafe(Ko's is OK but something else is needed further up the street	Oct 29, 2012 3:51 PM
100	restaurant	Oct 29, 2012 1:24 PM
101	KEEP THE LIBRARY	Oct 29, 2012 12:42 PM
102	Restaurant. A decent one	Oct 29, 2012 11:50 AM
103	Better pedestrian crosswalks	Oct 29, 2012 11:24 AM

Page 5, Q23. What facilities or services would you like to see added Downtown?

104	outdoor cafes	Oct 29, 2012 11:08 AM
105	Traffic circles	Oct 29, 2012 11:02 AM
106	Waterfront walking paths/boat marina	Oct 29, 2012 10:11 AM
107	Bus system	Oct 29, 2012 7:35 AM
108	waterfront development	Oct 29, 2012 6:30 AM
109	parks	Oct 29, 2012 6:27 AM
110	walking trail	Oct 29, 2012 5:50 AM
111	Get rid of the tavern	Oct 29, 2012 5:07 AM
112	Move forward with the marina project	Oct 29, 2012 4:05 AM
113	Parkades are an eyesore...	Oct 29, 2012 4:00 AM
114	recreational facilities for children	Oct 29, 2012 3:52 AM
115	additional cafes/eateries	Oct 29, 2012 3:48 AM
116	more emphasis on the value of river frontage for recreation	Oct 29, 2012 3:35 AM
117	place for good coffee	Oct 28, 2012 2:35 PM
118	place to gather	Oct 28, 2012 6:54 AM
119	public transit	Oct 27, 2012 4:38 PM
120	Seniors facility or gathering place	Oct 27, 2012 11:53 AM
121	More attractive store fronts	Oct 27, 2012 9:30 AM
122	outside cafe in summer	Oct 27, 2012 8:49 AM
123	replace the south parkade with angle parking	Oct 27, 2012 7:43 AM
124	more shopping, clothing, gift,craft or galleries	Oct 27, 2012 7:02 AM
125	Make King street a one way street, knock down the parkade and encourage nice shops for tourists and residents	Oct 27, 2012 4:43 AM
126	A dept store to replace Zellers	Oct 27, 2012 2:46 AM
127	better parking	Oct 26, 2012 5:01 PM
128	Boardwalk along river	Oct 26, 2012 4:28 PM
129	at least a good coffee shop that can afford to stay and continue as a gathering place.	Oct 26, 2012 2:33 PM

Page 5, Q23. What facilities or services would you like to see added Downtown?		
130	restaurant	Oct 26, 2012 2:28 PM
131	nice restaurant or two	Oct 26, 2012 2:13 PM
132	Street scape that reflects a 'we care attitude'	Oct 26, 2012 1:18 PM
133	better restaurants	Oct 26, 2012 12:59 PM
134	Cultural arts theatre	Oct 26, 2012 11:35 AM
135	more small business	Oct 26, 2012 11:30 AM
136	trash containers	Oct 26, 2012 11:15 AM
137	More pedestrian friendly / human scale infrastructure - better sidewalks / trails / safe crossing areas / street furniture, etc	Oct 26, 2012 11:03 AM
138	general facelift of town owned infrastructure	Oct 26, 2012 10:54 AM
139	nice restaurant	Oct 26, 2012 10:40 AM
140	Clean up, painting, better landscaping.	Oct 26, 2012 9:59 AM
141	access to the River on both sides	Oct 26, 2012 9:36 AM
142	Public Washrooms	Oct 26, 2012 6:24 AM
143	Maintenance - litter, weeds, snow clearing	Oct 26, 2012 5:36 AM
144	youth,community center	Oct 25, 2012 10:53 PM
145	Formal town square with multi-use open space	Oct 24, 2012 9:17 AM
146	Festival, event or paly space along the riverbanks (ideally where the parkades are now)	Oct 24, 2012 3:50 AM
147	Medical Clinic to also deal with new patients moving to Town from out of Province	Oct 23, 2012 1:47 PM
second priority		
1	get rid of the hideous parking platform	Jan 17, 2013 4:52 AM
2	slower vehicles; pedestrian cross walk mid-King St.	Dec 27, 2012 6:03 AM
3	dining facilities	Dec 6, 2012 6:44 PM
4	better kept buildings	Dec 2, 2012 6:04 PM
6	improve the look of King Street	Nov 28, 2012 7:54 PM
7	Bike Lane	Nov 28, 2012 4:01 PM
9	butcher shop, European style deli, other decent food offerings - very limited	Nov 26, 2012 1:01 AM

Page 5, Q23. What facilities or services would you like to see added Downtown?		
	cafes at present	
10	cafe/coffee shop	Nov 25, 2012 6:06 PM
11	appearence of store fronts-weeding not done,	Nov 25, 2012 10:25 AM
12	Improved sidewalks	Nov 23, 2012 7:08 AM
13	Entertainment	Nov 23, 2012 6:00 AM
14	entertainment	Nov 23, 2012 4:39 AM
17	Wayfaring/signage program	Nov 22, 2012 9:44 AM
18	Bike Lane	Nov 22, 2012 7:13 AM
19	Restaurants	Nov 19, 2012 8:28 PM
20	public transport	Nov 19, 2012 6:55 PM
21	Marina	Nov 19, 2012 4:28 PM
22	cafe	Nov 19, 2012 3:57 PM
23	artist studio/gallery spaces	Nov 19, 2012 10:01 AM
24	Performance centre	Nov 19, 2012 9:05 AM
25	Better crosswalk signage in front of Cummings	Nov 19, 2012 5:14 AM
26	taking advantage of the river and river bank	Nov 18, 2012 4:22 PM
27	trail path	Nov 18, 2012 12:53 PM
28	New bold signage on all commercial establishments	Nov 18, 2012 2:48 AM
29	more affordable living	Nov 17, 2012 6:48 PM
30	Bike lanes	Nov 16, 2012 2:09 PM
31	Get a bike lane designated for the bridge or make one side pedestrian and one cyclists	Nov 16, 2012 1:44 PM
32	boat tours for tourists, kayak rentals, etc.	Nov 16, 2012 1:11 PM
33	removal of parkade so river can be accessed	Nov 16, 2012 12:26 PM
34	Access to the river for walking/biking	Nov 16, 2012 12:17 PM
35	more inviting to people	Nov 16, 2012 11:54 AM
36	parkade development	Nov 16, 2012 11:32 AM
37	outdoor shopping	Nov 16, 2012 7:58 AM

Page 5, Q23. What facilities or services would you like to see added Downtown?

38	specialty (upscale) shops	Nov 14, 2012 8:22 AM
39	Green spaces	Nov 13, 2012 11:31 AM
40	cultural/arts centre	Nov 13, 2012 8:30 AM
41	Splash pad	Nov 13, 2012 8:08 AM
42	coffee shops	Nov 10, 2012 6:33 AM
45	more trees	Nov 8, 2012 2:55 PM
47	restaurants	Nov 8, 2012 6:45 AM
48	Bike trails	Nov 7, 2012 2:04 PM
49	Access and views of the river (remove South Parkade)	Nov 7, 2012 1:22 PM
50	eating areas	Nov 7, 2012 12:06 PM
51	better sidewalks	Nov 7, 2012 11:18 AM
52	better sidewalks	Nov 7, 2012 7:22 AM
54	More outdoor common areas	Nov 6, 2012 9:56 AM
55	parking meters that work	Nov 6, 2012 7:00 AM
56	outdoor gathering places	Nov 5, 2012 12:58 PM
57	keep library in central location	Nov 5, 2012 11:14 AM
58	a clothing store	Nov 3, 2012 1:04 PM
60	attractive dining	Nov 3, 2012 5:44 AM
62	Brighter coloured buildings	Nov 2, 2012 6:11 PM
63	cross walk by royal bank	Nov 2, 2012 5:37 PM
64	children focussed - natural playground, more park areas	Nov 2, 2012 4:10 PM
65	music store (instruments, records)	Nov 2, 2012 3:28 PM
66	concrete skateboard park	Nov 2, 2012 3:05 PM
67	bike lanes	Nov 2, 2012 2:15 PM
68	Variety of shops	Nov 2, 2012 2:15 PM
69	Restaurants and Cafes that serve healthy food	Nov 2, 2012 1:56 PM
70	more festivals and events	Nov 2, 2012 7:52 AM

Page 5, Q23. What facilities or services would you like to see added Downtown?

71	outdoor dining	Nov 1, 2012 3:37 PM
73	get rid of empty buildings	Nov 1, 2012 10:46 AM
74	business-oriented infrastructure: Free Wi-Fi and affordable 10-gigabit network for downtown business	Nov 1, 2012 9:42 AM
75	Nice Restaurants	Nov 1, 2012 4:46 AM
77	Free parking	Nov 1, 2012 4:07 AM
78	Malls operate Colaboratively, shared Shuttles to and from their Malls	Nov 1, 2012 3:00 AM
79	good parking nearby, but not taking up riverside space	Nov 1, 2012 2:34 AM
80	Off King St. parking	Oct 31, 2012 5:02 PM
81	Spruce up all of the heritage storefronts and heritage buildings in this town. It is a shame that the heritage buildings are ignored here.	Oct 31, 2012 3:39 PM
82	access to a farmers market to see local food and artisans	Oct 31, 2012 3:34 PM
83	good coffee shop (not Tim's)	Oct 31, 2012 3:10 PM
84	music venue	Oct 31, 2012 2:30 PM
85	Public transit	Oct 31, 2012 2:00 PM
86	Work with LaHave Marina and Riverport Marina to improve on river business qualities	Oct 31, 2012 1:18 PM
87	Revitalize King St - more shops, stop locating new businesses elsewhere. Big challenge	Oct 31, 2012 1:08 PM
88	aesthetic appeal to most downtown areas, both residential and town property and shops	Oct 31, 2012 12:39 PM
90	I'm excited about Cafe 12 and I hope that's going to be a great addition to downtown	Oct 31, 2012 12:21 PM
91	Edible gardens and disappearance of excessive lawns that leach chemicals and nutrients via runoff into the River	Oct 31, 2012 12:00 PM
92	Wider Side walks	Oct 31, 2012 11:55 AM
93	encourage residential development downtown. Also consider building size in appropriate locations. In view of Sustainable Bridgewater, rather than continue to expand outward, perhaps consider adjusting building height restrictions in appropriate areas, conserving space/limiting urban sprawl, while increasing population density. If downtown pop. density increases, this means more people will the opportunity to walk rather than drive to some destinations, also tying into Active Bridgewater	Oct 31, 2012 6:23 AM
94	Live music and entertainment events. Lunch hour Monday to Saturday?	Oct 30, 2012 1:30 PM

Page 5, Q23. What facilities or services would you like to see added Downtown?

95	Public space	Oct 30, 2012 6:56 AM
96	Encourage store & building owners to maintain their properties but in a way that enhances their historic features	Oct 30, 2012 4:14 AM
97	use spaces that are vacant would also be a huge help	Oct 29, 2012 7:06 PM
98	outdoor water fountains	Oct 29, 2012 4:06 PM
99	clothing stores/boutiques	Oct 29, 2012 3:51 PM
100	outdoor cafe	Oct 29, 2012 1:24 PM
101	KEEP THE LIBRARY	Oct 29, 2012 12:42 PM
102	Nice shops	Oct 29, 2012 11:50 AM
103	Playground	Oct 29, 2012 11:24 AM
104	pedestrian area	Oct 29, 2012 11:08 AM
105	Replace King Street Parking lots	Oct 29, 2012 11:02 AM
106	Better shops and resturants	Oct 29, 2012 10:11 AM
107	Fine outdoors dining	Oct 29, 2012 7:35 AM
108	wider roads and bridges	Oct 29, 2012 6:30 AM
109	different types of stores, needs more variety	Oct 29, 2012 6:27 AM
110	business clean up, store front clean up (Sagors is the latest example)	Oct 29, 2012 5:50 AM
111	Upscale restaurant / lounge	Oct 29, 2012 5:07 AM
112	removal of the parkades to allow for development along the riverbank	Oct 29, 2012 4:05 AM
113	Widen sidewalks	Oct 29, 2012 4:00 AM
114	sidewalk condition	Oct 29, 2012 3:52 AM
115	additional shopping	Oct 29, 2012 3:48 AM
116	more bike lanes	Oct 29, 2012 3:35 AM
118	open space	Oct 28, 2012 6:54 AM
119	beautify	Oct 27, 2012 4:38 PM
121	sidewalk cafes with outdoor summer seating	Oct 27, 2012 9:30 AM
123	open the mall to the river	Oct 27, 2012 7:43 AM
124	more cafes, restaurants	Oct 27, 2012 7:02 AM

Page 5, Q23. What facilities or services would you like to see added Downtown?

125	Paint the shipyard's landing bandstand	Oct 27, 2012 4:43 AM
128	More bistro/restaurants/cafe.	Oct 26, 2012 4:28 PM
129	a restaurant with traditional fare	Oct 26, 2012 2:33 PM
130	library	Oct 26, 2012 2:28 PM
131	store owners encouraged through possible grants to spruce up their facades possibly bring them back to the 40-50sfacades of the	Oct 26, 2012 2:13 PM
132	Traffic patterns that make sense: one way	Oct 26, 2012 1:18 PM
134	Cafes	Oct 26, 2012 11:35 AM
135	bus service	Oct 26, 2012 11:30 AM
136	public seating	Oct 26, 2012 11:15 AM
137	More diverse shopping & restaurant experiences	Oct 26, 2012 11:03 AM
140	View to river improved especially at parkades.	Oct 26, 2012 9:59 AM
141	more restaurants/bars	Oct 26, 2012 9:36 AM
143	More shops	Oct 26, 2012 5:36 AM
145	More public boating infrastructure (floating wharfs, etc.)	Oct 24, 2012 9:17 AM
146	More cafes, restuarants, stores that people can casually stop into when visiting the downtown.	Oct 24, 2012 3:50 AM
147	GoodLife Fitness	Oct 23, 2012 1:47 PM
third priority		
1	make the mallnotlook dreadful	Jan 17, 2013 4:52 AM
2	many more trash receptacles	Dec 27, 2012 6:03 AM
3	outdoor market	Dec 6, 2012 6:44 PM
6	usage of water front i.e. parkades are an eyesore	Nov 28, 2012 7:54 PM
7	Bus Service	Nov 28, 2012 4:01 PM
9	encouraging pedestrians	Nov 26, 2012 1:01 AM
11	better mowing done	Nov 25, 2012 10:25 AM
12	Increased light	Nov 23, 2012 7:08 AM
13	Dining	Nov 23, 2012 6:00 AM

**Page 5, Q23. What facilities or services would you like to see added Downtown?**

14	cafes	Nov 23, 2012 4:39 AM
17	Increased mix of retail opportunities	Nov 22, 2012 9:44 AM
18	More of a park setting	Nov 22, 2012 7:13 AM
19	Convenience store	Nov 19, 2012 8:28 PM
20	having landlords fix facades or destroy unsightly premises	Nov 19, 2012 6:55 PM
21	Late night coffee shop/bistro/jazz bar	Nov 19, 2012 4:28 PM
22	permanent home for farmers market	Nov 19, 2012 3:57 PM
25	Decent coffee shop on King Street	Nov 19, 2012 5:14 AM
28	attract a one or two restaurants	Nov 18, 2012 2:48 AM
29	green space incorporated into the buildings IE rooftop gardens	Nov 17, 2012 6:48 PM
30	Better selection of shops	Nov 16, 2012 2:09 PM
31	remove the fountain and bring back the natural beauty/landscape of downtown	Nov 16, 2012 1:44 PM
35	greenspace	Nov 16, 2012 11:54 AM
36	outdoor cafe	Nov 16, 2012 11:32 AM
37	park area	Nov 16, 2012 7:58 AM
40	free community space that can be used by community groups for meetings	Nov 13, 2012 8:30 AM
41	More residential units	Nov 13, 2012 8:08 AM
42	entertainment	Nov 10, 2012 6:33 AM
45	bicycle path	Nov 8, 2012 2:55 PM
47	signs indicating there is a viable park area by the bridges	Nov 8, 2012 6:45 AM
49	Placemaking--see: <a href="http://www.cityrepair.org">www.cityrepair.org</a>	Nov 7, 2012 1:22 PM
50	better use of the river with boat tours, activities	Nov 7, 2012 12:06 PM
51	lights in parkade	Nov 7, 2012 11:18 AM
52	playground	Nov 7, 2012 7:22 AM
54	More patios with food/coffe	Nov 6, 2012 9:56 AM
56	good community bulletin board space	Nov 5, 2012 12:58 PM
57	do away with parking meters	Nov 5, 2012 11:14 AM

**Page 5, Q23. What facilities or services would you like to see added Downtown?**

60	more shops,	Nov 3, 2012 5:44 AM
62	Live music venue	Nov 2, 2012 6:11 PM
63	signs on polls have some order not on hear and there	Nov 2, 2012 5:37 PM
64	diversity in restaurants...too much junk food.	Nov 2, 2012 4:10 PM
65	access nova scotia	Nov 2, 2012 3:28 PM
66	concrete skateboard park	Nov 2, 2012 3:05 PM
67	restaurants/coffe shops	Nov 2, 2012 2:15 PM
68	Coffee Shops	Nov 2, 2012 2:15 PM
69	Alternative food shops where you can buy stuff that's different than the grocery store fare.	Nov 2, 2012 1:56 PM
70	facelift properties	Nov 2, 2012 7:52 AM
71	dog friendly areas	Nov 1, 2012 3:37 PM
73	force landlords to clean up buildings	Nov 1, 2012 10:46 AM
74	pedestrian-oriented walks and malls, possibly covered, with view along the waterfront	Nov 1, 2012 9:42 AM
75	boat docks where there already is, but with lighting	Nov 1, 2012 4:46 AM
78	Two (2) more Seniors Wheels Vehicles	Nov 1, 2012 3:00 AM
80	Businesses that front onto the river eg restaurants	Oct 31, 2012 5:02 PM
81	The garbage around town is disgusting. Even on the trails it is shameful and there are no garbages that are easily accessable, no recycling initiatives in the schools, and no community cleanup days.	Oct 31, 2012 3:39 PM
82	public transit	Oct 31, 2012 3:34 PM
84	more family friendly activities	Oct 31, 2012 2:30 PM
85	Higher end restaurants	Oct 31, 2012 2:00 PM
88	Recreation services	Oct 31, 2012 12:39 PM
90	I love the farmer's market on Saturday and think the town should help support it financially (more if they already do so) so they can afford things like promotion and paying for musicians	Oct 31, 2012 12:21 PM
91	Preserved marshlands and reforested land for public enjoyment and ecosystem services	Oct 31, 2012 12:00 PM
92	Transit system	Oct 31, 2012 11:55 AM

Page 5, Q23. What facilities or services would you like to see added Downtown?		
94	A	Oct 30, 2012 1:30 PM
95	Cultural elements - artistic expressions, like recent bench project and banners	Oct 30, 2012 6:56 AM
96	Encourage small-scale shops and cafes	Oct 30, 2012 4:14 AM
97	parking	Oct 29, 2012 7:06 PM
98	more shops	Oct 29, 2012 4:06 PM
99	a branch of the new library. The new site is inconvenient or even impossible for many.	Oct 29, 2012 3:51 PM
101	KEEP THE LIBRARY	Oct 29, 2012 12:42 PM
103	More artisans/cultural opportunities	Oct 29, 2012 11:24 AM
104	activities for families and children	Oct 29, 2012 11:08 AM
105	Walking trails in town along river where possible	Oct 29, 2012 11:02 AM
107	More sophisticated entertainment (dancing, concerts, ect)	Oct 29, 2012 7:35 AM
108	north street widened	Oct 29, 2012 6:30 AM
109	lighting	Oct 29, 2012 6:27 AM
110	biking lanes and racks	Oct 29, 2012 5:50 AM
112	cafes, bistros along King St / riverfront	Oct 29, 2012 4:05 AM
113	Improve parking	Oct 29, 2012 4:00 AM
116	build public buildings on the waterfront with river views and public access	Oct 29, 2012 3:35 AM
118	access to river	Oct 28, 2012 6:54 AM
119	public washroom	Oct 27, 2012 4:38 PM
121	better litter control	Oct 27, 2012 9:30 AM
123	establish a park/boat access under the south parkade	Oct 27, 2012 7:43 AM
124	public washrooms	Oct 27, 2012 7:02 AM
128	More garbage/recycling and more frequent pickup.	Oct 26, 2012 4:28 PM
129	freedom to support young entepeneurs at least on the parkway to and make it attractrive for support of young people whether through music, small business ventures without charging exorbitant fees.	Oct 26, 2012 2:33 PM
130	arts & craft	Oct 26, 2012 2:28 PM
131	encouragement of bikes through parking and lanes	Oct 26, 2012 2:13 PM

Page 5, Q23. What facilities or services would you like to see added Downtown?		
132	Return waterfront to waterfront (get rd of parkcade	Oct 26, 2012 1:18 PM
134	canoe races, paddle boats	Oct 26, 2012 11:35 AM
135	pedestrian cross walks	Oct 26, 2012 11:30 AM
137	Beautification (trees, art work, flowers, etc)	Oct 26, 2012 11:03 AM
140	Improvements of buildings.	Oct 26, 2012 9:59 AM
141	more vegetation	Oct 26, 2012 9:36 AM
143	Restaurants	Oct 26, 2012 5:36 AM
145	More boutique style shops and restaurants	Oct 24, 2012 9:17 AM
146	More parks and public sapces.	Oct 24, 2012 3:50 AM
147	More Restaurants	Oct 23, 2012 1:47 PM
fourth priority		
1	greenspace	Jan 17, 2013 4:52 AM
2	bicycle lanes	Dec 27, 2012 6:03 AM
6	slow traffic, possibly make it a one way street	Nov 28, 2012 7:54 PM
7	Better looking store fronts	Nov 28, 2012 4:01 PM
12	facade improvements	Nov 23, 2012 7:08 AM
13	Unique shops	Nov 23, 2012 6:00 AM
14	clean looking buildings	Nov 23, 2012 4:39 AM
18	More restaurants	Nov 22, 2012 7:13 AM
21	Bus route	Nov 19, 2012 4:28 PM
25	Lower the parking levy for businesses in the downtown, a start would be tendering of snow removal to find a better price they are out there	Nov 19, 2012 5:14 AM
28	use the beautiful LaHave to full advantage for god's sake	Nov 18, 2012 2:48 AM
29	Bike routes with parking and shared car services	Nov 17, 2012 6:48 PM
31	stick to natural landscape and historic and promote businesses to do the same	Nov 16, 2012 1:44 PM
35	playground	Nov 16, 2012 11:54 AM
37	more waterfront ammenities	Nov 16, 2012 7:58 AM
40	public transit to get around	Nov 13, 2012 8:30 AM

Page 5, Q23. What facilities or services would you like to see added Downtown?

41	Public transit	Nov 13, 2012 8:08 AM
49	Attention to the historical aspects/buildings on King street	Nov 7, 2012 1:22 PM
50	small businesses that attract more people	Nov 7, 2012 12:06 PM
51	no parking meters	Nov 7, 2012 11:18 AM
52	water park	Nov 7, 2012 7:22 AM
54	More night life	Nov 6, 2012 9:56 AM
60	wine store	Nov 3, 2012 5:44 AM
62	Trendy restaurants	Nov 2, 2012 6:11 PM
64	too bad the library couldn't stay downtown somewhere - its the only reason my kids want to go downtown - oh well	Nov 2, 2012 4:10 PM
66	concrete skateboard park	Nov 2, 2012 3:05 PM
68	Restaurants	Nov 2, 2012 2:15 PM
69	ANYTHING for the teenagers.	Nov 2, 2012 1:56 PM
71	sitting areas	Nov 1, 2012 3:37 PM
73	easier accessibility	Nov 1, 2012 10:46 AM
74	greenery and marine access	Nov 1, 2012 9:42 AM
75	clean it up	Nov 1, 2012 4:46 AM
80	TREES	Oct 31, 2012 5:02 PM
81	Less takeouts/corporate junk franchises and more small businesses. There is no appeal for residence in Bridgewater. None. I live here because I have to. This place is the dogs ass.	Oct 31, 2012 3:39 PM
82	outdoor cafe along the river	Oct 31, 2012 3:34 PM
84	more festivals	Oct 31, 2012 2:30 PM
85	Open plaza or mall with street performers and vendors market	Oct 31, 2012 2:00 PM
88	dining	Oct 31, 2012 12:39 PM
90	I think something needs to be done about rent and/or taxation to make King Street thrive with businesses, like it used to...	Oct 31, 2012 12:21 PM
91	Infrastructure for a public bus system, safe bicycle lanes, and safer, wider sidewalks for longboarding!	Oct 31, 2012 12:00 PM
92	Locally owned business/non-franchise	Oct 31, 2012 11:55 AM

Page 5, Q23. What facilities or services would you like to see added Downtown?

94	A	Oct 30, 2012 1:30 PM
97	king streetcourt is great like to see a board walk with small shops	Oct 29, 2012 7:06 PM
98	outdoor eating venues...patio seating	Oct 29, 2012 4:06 PM
99	something to replace the motorcycle gang facility	Oct 29, 2012 3:51 PM
101	KEEP THE LIBRARY	Oct 29, 2012 12:42 PM
103	?	Oct 29, 2012 11:24 AM
105	Establish truck routes	Oct 29, 2012 11:02 AM
107	A return to better store front façades. Go back to the Victorian style façades. Its more charming	Oct 29, 2012 7:35 AM
109	outside seating	Oct 29, 2012 6:27 AM
110	south parkade gone	Oct 29, 2012 5:50 AM
112	kayak rentals	Oct 29, 2012 4:05 AM
113	Promote more types of business	Oct 29, 2012 4:00 AM
118	niche shopping	Oct 28, 2012 6:54 AM
123	roof-top garden on the mall	Oct 27, 2012 7:43 AM
128	Pedestrian signage of shops available.	Oct 26, 2012 4:28 PM
129	enhance the parkway as a good usable space where it can be used for parking and entertainment, Bridgewater the mainstreet of the southshore should become a promotion town, farmers market, Lunenburg is way ahead, why is that?	Oct 26, 2012 2:33 PM
132	Festivals, concerts	Oct 26, 2012 1:18 PM
134	better sidewalks	Oct 26, 2012 11:35 AM
137	Performing arts venue	Oct 26, 2012 11:03 AM
140	Lighting, greening and clean up of under parkades.	Oct 26, 2012 9:59 AM
141	more housing	Oct 26, 2012 9:36 AM
143	Keep the Parking	Oct 26, 2012 5:36 AM
145	More pedestrian/bicycle friendly amenities and infrastructure	Oct 24, 2012 9:17 AM
146	Storefront improvement programs of some kind	Oct 24, 2012 3:50 AM
147	Water Garden	Oct 23, 2012 1:47 PM

**Page 6, Q26. What improvements/design elements would you like to see included in a waterfront design**

1	Improve the appearance of the parkades, with murals, lighting, flowers. We need them for the businesses to stay in the downtown!	Nov 19, 2012 8:46 PM
2	Color and welcoming design	Nov 19, 2012 6:59 PM
3	Look, why harp on parking when the rest of the world is going the other way - there is so much parking on the other side of the bridge is like an ocean of cars - let people walk over to the other side on the two bridges - the "mall" side is such a blight, at least make the store side of the river wonderful. It is so frustrating to see such a lack of vision for the place.	Nov 18, 2012 2:57 AM
4	Uniform commercial signage	Nov 13, 2012 11:36 AM
5	Revitalized and usable town wharf.	Nov 8, 2012 2:26 PM
6	Urban art	Nov 7, 2012 1:27 PM
7	museums, more emphasis on heiritage	Nov 7, 2012 12:14 PM
8	public washrooms not prtapotties	Nov 6, 2012 7:06 AM
9	Skatepark	Nov 5, 2012 8:01 AM
10	waterfront green development is key	Nov 3, 2012 3:30 AM
11	farmers market	Nov 2, 2012 7:52 PM
12	Local artisan market	Nov 2, 2012 6:13 PM
13	concrete skateboard park	Nov 2, 2012 3:07 PM
14	Paint the store fronts. Have 6 colors and merchant paint their building in one of those colors.Give Bridgewater the character of the South Shore like Mahone Bay and Lunenburg.The South Shore is known for being colorful,cheerful and historic.	Nov 1, 2012 12:25 PM
15	Many small efficient insulated retail spaces suitable for year-round artisan booths or studios, which can be easily opened to the public on good days all year round	Nov 1, 2012 9:50 AM
16	accessible farmers market to be able to purchase local goods	Oct 31, 2012 3:37 PM
17	Public transportation	Oct 31, 2012 2:04 PM
18	new bar for young adults!	Oct 31, 2012 12:30 PM
19	Public Transit and more bike/skateboard/inline skate friendly paths and areas	Oct 31, 2012 12:26 PM
20	We must restore riparian buffer zones along the banks of the LaHave River with native species to protect our river from erosion and the degradation of water quality! This must be a priority for future sustainability!	Oct 31, 2012 12:04 PM
21	Drinking Fountain for refillable bottles/hydration	Oct 31, 2012 12:03 PM
22	Hire someone who can help stores design attractive and inviting store fronts.	Oct 30, 2012 1:37 PM

**Page 6, Q26. What improvements/design elements would you like to see included in a waterfront design**

	BACC, BDA or King Street Merchants Association can pay for it.	
23	Fishing areas.	Oct 28, 2012 6:51 AM
24	upscale waterfront restaurant. You can't "dine" in Bridgewater and that's why we all have to go to Lunenburg to have some level of sophistication	Oct 27, 2012 7:47 AM
25	walking bridge	Oct 27, 2012 2:50 AM
26	Safe area or phones with direct line for emergency. Better lighting.	Oct 26, 2012 4:33 PM
27	If there was angle parking from Dominion to Dufferin on the wwest side of the street and the south parkade was gone there might be a whole different ambiance .	Oct 26, 2012 2:20 PM
28	Downtown needs a central gathering area (e.g., town square where southern parkade is currently located?)	Oct 24, 2012 9:22 AM
29	Water Garden	Oct 23, 2012 2:01 PM

Page 6, Q27. Please add any additional comments you may have.

1	This project is a complete waste of tax payers money. When I walk downtown there are many old vacant buildings and basically only thre corporate businesses. Look at what is taking place in the Mall vacant space upon vacnat space. It is nice to look outside the box, however, when the Town has to use approximately \$82,000.00 from the reserve fund to bvlance this fiscal years budget we need to look and what we can aford and what we cannot aford as tax payers. At this time, we don"t even now what the cost is going to be to manage the MUlit Purpose Facility. This whole vision to bring back down town to were it was in the past is pie in the sky. I do two things downtown, go to the bank and buy groceries, the rest of my time purchasing is done outside of the downtown area. The statement by Jlll Robertson on November 22, 2012 at the firehall about our children having to pay for this project was uncalled for. I certainly do not what to put this burden on the taxpayers of trhe future. There is a saying that we must look outside the box. However, it is time this new council looks inside the box and review our current committements and what does Bridgewater really need.	Nov 30, 2012 9:18 AM
2	We need better roads! - 2 lanes.	Nov 28, 2012 4:04 PM
3	nice cafes are important, but please no more trashy food places	Nov 22, 2012 11:56 AM
4	I know there is a lot of talk about taking down the parkades, starting with the South parkade. I have been told that it is going to happen. So I feel that this process is just "going through the motions" to be able to say we consulted with the public etc. and this is what the people wanted. Even from the way the survey is worded leads me to believe that because I know from the BDAs five phase plan that some o f these things listed above are what they plan to do with the space after it is torn down. Show support for the downtown merchants and people who patronize them... Fix the parkades up so p	Nov 19, 2012 8:46 PM
5	Why use the word "canteen" - why not decent cafe or decent restaurant - there are plenty of people who live around B'Water in other small towns who would gladly go there if there was something to go for besides mall shopping and even that is depressing. We go to the downtown a couple of times a week from nearby - we rarely go over the bridge to the store side unless we are going to the library because there is no "draw" to that side - unfortunately then we are left to shop at big box or a sprawling mall and neither have any imagination and only fast food. People would spend money there if it was a pleasant experience to be there. You can't be all about the malls - there has to be a vital ATTRACTIVE downtown.	Nov 18, 2012 2:57 AM
6	Ideally the downtown will become more of a green zone with historic landscape kept as a priority. It doesn't need to be jazzed up with inefficient, costly and unsightly items such as the fountain. The fountain takes away from the natural setting of the town. No more "modern elements" please....	Nov 16, 2012 1:50 PM
7	Need to fill the vacant storefronts on King Street versus building new.	Nov 16, 2012 9:45 AM
8	In order to attract businesses the commercial tax rate must be lowered	Nov 14, 2012 8:27 AM
9	King Street businesses need to find another location for their garbage, compost bins, CARDBOARD, pick-up, etc. I've had to navigate around many piles of cardboard and garbage scattered along the sidewalk.	Nov 13, 2012 8:27 AM

Page 6, Q27. Please add any additional comments you may have.

10	I would like to see the parkades gone and more parking behind king street or more bicylce pathl would also like to see a rowing club or paddling club on our river	Nov 8, 2012 3:01 PM
11	I believe the town needs to help merchants open new businesses. Good restaurants in the downtown area will attract more people. As it stands now, when tourists (who should be an important part of this equation) ask what there is to see in downtown Bridgewater, and where is a good place to eat, the choices are very few. Support for the local merchants, and lower rents on buildings, will help a lot!	Nov 8, 2012 6:49 AM
12	the parkades need to stay,there is no where else for parking. changing the parking to both sides of the street and angle parking would create traffic problems	Nov 8, 2012 5:14 AM
13	Maintenace of commercial buildings should be part of a bylaw. I don't know if the Town can perform the maintenance for the owners and put a lien on their property but that would be an idea.	Nov 7, 2012 2:08 PM
14	Watch the place-making video: <a href="http://www.youtube.com/watch?feature=player_embedded&amp;list=PLg5yavXHiDwK4KkIWtIXw8JAM_PMsZWK5&amp;v=8uh1u12SqRo">http://www.youtube.com/watch?feature=player_embedded&amp;list=PLg5yavXHiDwK4KkIWtIXw8JAM_PMsZWK5&amp;v=8uh1u12SqRo</a> a project that I was part of this summer. Also check out Ripples2waves.wordpress.com Part of what needs to happen is a changing of the story of Bridgewater to a place of vitality and resilience. We've got it all really we just need reminders that we do have it all. thanks for you work on this great project!	Nov 7, 2012 1:27 PM
15	I think we need to consider a more European aproach to life. Times are changing and we need to consider that the young people/families of today want a more healthy, sustainable lifestyle and want to enjoy the beautiful river and community. They want to connect with each other in a way that incorporates these things.	Nov 6, 2012 10:04 AM
16	bridgewater business need easy access for the elderly parking in the downtown area is a must	Nov 5, 2012 11:19 AM
17	As a 20 year old male who has lived in bridgewater all his life, I have seen many of the youth in the town exploit the use of drugs and in turn cause many issues that could be avoided with an opportunity for recreation. Yes, it is up to them to utilize a feature like a skatepark, but as the south shores largest and most populated town to not even have a couple of ramps or anything resembling a skate park is almost embarrassing.	Nov 5, 2012 8:01 AM
18	take some risks and creat a river centred core area	Nov 3, 2012 3:30 AM
19	traffic flow analusis should be done to determine wetherking st. can be turned into a one way rd from rofies to dufferin southbound, creating diagonal parking for the unidirectional traffic, maintaining two directions on the old bridge but eliminating the parkade and developing a river walk/ development zone with commercial space one both sides of the street, it would serve to condense the shipping area, and create a coherent downtown core.	Nov 2, 2012 7:52 PM
20	Downtowns were built in a time with mixed land use, lots of walking to and	Nov 2, 2012 4:14 PM

Page 6, Q27. Please add any additional comments you may have.

	fro...whatever come of it has to be the exact opposite of the big box store business park experience	
21	all of these improvements or design elements that are listed here would be good ideas if the apparent drug and crime problems are dealt with first. for example, building a beautiful park is lovely only if residents feel safe using it. a more useful police presence and better addiction services would be in your best interest. as a rural resident, brigewater is a place i go to only because i have to, never because i want to.	Nov 2, 2012 3:37 PM
22	concrete skateboard park	Nov 2, 2012 3:07 PM
23	How can we get rid of all the concrete? And the mall with it's back to the river... how did that happen?	Nov 2, 2012 2:00 PM
24	Same as above.	Nov 1, 2012 12:25 PM
25	The survey does not seem ambitious enough. The parkades space alone is sufficient to support a pedestrian mall with river access all along the waterfront and rooftop gardens/restaurants with excellent views - the river view from the south side of King need not be preserved - marina and other structures can be built out over the water also. Part-time storefronts are fine if they are small and physically appealing and have affordable utilities/connectivity. Opportunistic openings rather than "seasonal" planning should be the rule with our unpredictable weather, meaning, more smaller shops open sporadically (with good notification of opening on the Internet). Marina and bridge options also determine most of the other alternatives possible. Could King Street even be covered over someday with a broad open plaza above the existing street? The limits of the study aren't clear.	Nov 1, 2012 9:50 AM
26	Downtown requires standard retail hours so working people can shop. More service type business like dentist, salon, doctor twho do not require walk in traffic but people we visit. Events such as "taste of" or "after glow" to get people visiting and keep retail open during these events.	Nov 1, 2012 4:42 AM
27	It's about time that these options are included in a survey about Bridgewater. This town is dead while Lunenburg and Mahone Bay are thriving. Revitalize this place by acknowledging our Heritage, not by destroying it for corporate franchise access and high density housing.	Oct 31, 2012 3:41 PM
28	I think the group that is putting together the proposal for Cafe in the library building will be great for the downtown core.	Oct 31, 2012 2:33 PM
29	I want to see a vibrant downtown Bridgewater and cannot wait to offer assistance in this regard.	Oct 31, 2012 2:04 PM
30	Public transport serving the downtown complements all the rest.	Oct 31, 2012 1:10 PM
31	I think there is enough parkland as it stands, and venues/amphitheatres. I love King's Court and Shipyard's landing. A play area would be great, maybe with a splashpad for family's who can't get out to the beach in the summertime.	Oct 31, 2012 12:26 PM
32	Please restore the banks of the LaHave River to improve the beauty and health	Oct 31, 2012 12:04 PM

Page 6, Q27. Please add any additional comments you may have.

	of Bridgewater and the health of the River and all those people who depend upon the River downstream!	
33	"bridewater has a strong and clearly defined identity" it does, and it is one that is NOT positive. Bridgewater is dirty, highly commercial, with a criminal undertone, it is not Pretty, Environmentally Concious, nor a place i would reccomend as a tourist destination. I live here and dread my trips into bridgewater from the Shore as it is so dirty, impoverished looking, with zero aesthetic/curb appeal. I am a cyclist, and i prefer to go out of my way and cycle to lunenburg and pay higher prices to avoid the depressing atmosphere of Bridgewater. Also, i would LOVE in the colder months to be able to leave my bike in the shed and hop on a bus to get into town and do my shopping, i hope that is a real consideration for our area, as it would be easier on the environment, our roads, our pocketbooks, our access to employment, and accessibility of our Aging and young populations! I am PLeased that this survey exists, and i hope it is a sign that positive change is coming to help save our sinking ship. Actions, Not words promote change, lets see this morph into actions!	Oct 31, 2012 12:03 PM
34	As indicated in section 24, I strongly 110% agree that higher residential density coupled with additional commercial or office development would be an asset to Bridgewater's downtown. We should strive to reach a place in which our downtown core is a place for work and recreation. To town planners, again as previously mentioned, as part of the town's Plannning Review, lets look at building height restrictions. I'm definitely not saying build a 10-storey apartment building in downtown, development needs to fit area, but if we can increase building height restrictions, where appropriate, then we're promoting better planning practices in terms of conserving land, but we're also increasing our density, a definite plus for the downtown core	Oct 31, 2012 6:34 AM
35	The south parkade has to go	Oct 31, 2012 4:04 AM
36	Pop Up Store Fronts. Encourage landlords of empty spaces to offer their empty space to a new business entrepreneur for free, excluding utilities, for a 3 to 1 year term. A busy and active street will only help enhance the value of his property, the people using the property may become paying tenants, entrepreneurs would have the opportunity to,try new things at lower risk.	Oct 30, 2012 1:37 PM
37	Out of provence visitors tell us that the appearance of King St., in general. makes them think the town is going downhill.	Oct 29, 2012 3:56 PM
38	I am not sure why the town feels it needs to move the Library to the LCLC. The residents who use the Library, myself included walk to the Library when we need it. The town doesn't have the infrastructure to handle anymore vehicle traffic, moving the Library will create extra traffic going through the town, it will take people out of the shopping area as well as the folks who use the Library computers to search for jobs won't be able to get to the Library if it is moved. Please keep the Library in the downtown. Everyone knows a LIBRARY IS THE CENTRE OF ANY TOWN.....Keep it the centre of Bridgewater.	Oct 29, 2012 12:48 PM
39	It would have been good to define the "downtown area". I wasn't sure, at first, whether that included the mall or not. I then decided that it did.	Oct 29, 2012 11:13 AM
40	King Street redesign should include wheel chair access from parking and to river	Oct 29, 2012 11:08 AM

Page 6, Q27. Please add any additional comments you may have.

	front if parcares are replaced	
41	A bus system would be idea for alot of people. You would draw in people from other comunities, like mahone bay or lunenburg. They don't have a movie theater or walmart. Some people don't drive this could really draw people in to our town. We also need things for mature entertainment. I'm married with kids, I don't want to go to a sleezy bar. I want to go to concerts and movies on the lawn as well as dancing under the stars. Fine dinnig should definatly be encourages in the town/ We don't want to have to go to lunenburg for a fine meal.	Oct 29, 2012 7:39 AM
42	Open the waterfront, more trees and nature, boat friendly. There is a river, use it.	Oct 28, 2012 6:51 AM
43	Any improvements would be welcomed.	Oct 27, 2012 8:51 AM
44	mall should open onto the river. Parking parkades should be beautified - they are extremely ugly. Store signage on Kind St should be standardized in some way - especially such as the Dicount place and Cleves and the Tops'l etc should be demolished. Bridgwater downtown needs some stricter building codes. There are too few people around town that remember what the area south of the old bridge looked like before the parking parkade was built - that south one is abhorrent and easy access to the river was always down the driveway by Ko's. Tear dwn that ugly old LaHave Furnishings store and make it a parking lot. Buy the Nauss property and turn it into a restaurant. Don't just do quick fixes.	Oct 27, 2012 7:47 AM
45	We have a beautiful river running directly through our town. We have decided the best use of this river is to block one side with a mall and the other with a parkade. It's never too late to correct these wrongs.	Oct 26, 2012 4:33 PM
46	As a property owner in the downtown I have been part of and have been waiting fosome 20 years for some improvement in the downtown but have seen very little meaningful results that would attract new businesses. I have spent hundreds of thousands to try to do my part only to have seen great improvements in my taxes. I hope this new study will not end up like most of the otherones, on the shelf. Much needs to be done on both side of the river of which I hear some talk lets see what is going to happen, New Mayor and 5 new councilours Lets go gang.	Oct 26, 2012 2:42 PM
47	congratulations to the town for at least starting this process. I hope it bears fruit.	Oct 26, 2012 2:20 PM
48	Businesses should be willing to take an active role in creating a streetscape that looks appealing. having standards for what it should look like is important.	Oct 26, 2012 1:21 PM
49	need lots of parking	Oct 26, 2012 1:00 PM
50	The focus of the revitalization and any branding exercise should be on the LaHave River. The river is the reason for the ToB's existence.	Oct 26, 2012 10:07 AM
51	some questions were too repetitive of each other, different #, basically same question worded a bit differently.	Oct 26, 2012 9:24 AM
52	I hear one way streets being mentioned - this would be very bad for tourists and businesses.	Oct 26, 2012 5:40 AM

Page 6, Q27. Please add any additional comments you may have.

53	Love Bridgewater and the work you are doing to enhance where we live!	Oct 26, 2012 4:38 AM
54	Whatever we do has to be reasonably affordable. This is a small town with limited resources. Identify achievable solutions and spend time developing the implementation strategy!	Oct 24, 2012 9:22 AM
55	Once we start developing our property on Lahave St it should complement well with the Town's Waterfront Master Plans	Oct 23, 2012 2:01 PM



# Preliminary Cost Estimate

## Phase One Key Projects (Gas Tax Funding)

Estimate of Probable Costs - Phase One				
	Measure	Units	Unit Cost	Cost
<b>Bridgewater Downtown Plan</b>				
<b>King Street (Old Bridge to Dufferin)</b>				
Hardscape Removal	3000	sq.m.	\$6	\$18,000
150mm Concrete Curb	310	lin.m.	\$150	\$46,500
Asphalt Road	175	lin.m.	\$110	\$19,250
Asphalt Paving	696	sq.m.	\$75	\$52,200
3m Concrete Sidewalk	128	lin.m.	\$200	\$25,600
Bump-Outs Hardscape	825	sq.m.	\$110	\$90,750
Rain Garden Plantings	200	sq.m.	\$90	\$18,000
Street Trees	15	ea.	\$750	\$11,250
Tree Grates	15	ea.	\$1,000	\$15,000
Benches	8	ea.	\$2,000	\$16,000
Line Painting	1	lump sum	\$4,000	\$4,000
Parkade Removal	1	lump sum	\$150,000	\$150,000
<b>Subtotal</b>				<b>\$466,550</b>
<b>Detailed Design (12%)</b>				<b>\$55,986</b>
<b>Contingency (15%)</b>				<b>\$69,983</b>
				<b>\$592,519</b>

<b>Old Bridge Landing Park</b>				
2m Concrete Ramp	57	lin.m.	\$160	\$9,120
2m Boardwalk	90	lin.m.	\$600	\$54,000
Deck Structure	1	lump sum	\$75,000	\$75,000
Lower Plaza Tinted Concrete Paving	586	sq.m.	\$90	\$52,740
2m Gabion Basket Retaining Structure - Shoreline	92	sq.m face	\$25	\$2,300
Gabion Basket Retaining Structure - Ramp	64	sq.m face	\$25	\$1,600
Boulders	50	ea.	\$350	\$17,500
Ramp Railing	57	lin.m.	\$800	\$45,600
Timber Seat Wall w/Conc. Footing	15	ea.	\$600	\$9,000
Timber Kick Rail	70	lin.m.	\$75	\$5,250
Soil & Sod	405	sq.m.	\$8	\$3,240
Topsoil	110	cub.m.	\$25	\$2,750
Trees	15	ea.	\$750	\$11,250
Shrubs	700	sq.m.	\$25	\$17,500
<b>Subtotal</b>				<b>\$306,850</b>
<b>Detailed Design (12%)</b>				<b>\$36,822</b>
<b>Contingency (15%)</b>				<b>\$46,028</b>
				<b>\$389,700</b>

<b>Lighting</b>				
King Street - Phase 1		Lump Sum		\$42,000
King Street - Phase 2		Lump Sum		\$42,000
Old Bridge Landing Park		Lump Sum		\$43,050
<b>Subtotal</b>				<b>\$127,050</b>
<b>Detailed Design (12%)</b>				<b>\$15,246</b>
<b>Contingency (15%)</b>				<b>\$19,058</b>
				<b>\$161,354</b>

<b>Signage</b>				
King Street - Phase 1		Lump Sum		\$34,550
King Street - Phase 2		Lump Sum		\$35,450
Old Bridge Landing Park		Lump Sum		\$5,200
<b>Subtotal</b>				<b>\$75,200</b>
<b>Detailed Design (12%)</b>				<b>\$9,024</b>
<b>Contingency (15%)</b>				<b>\$11,280</b>
				<b>\$95,504</b>
				<b>\$1,239,076</b>



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